



AGENDA

FOR COUNCIL ASSESSMENT PANEL MEETING TO BE HELD ON

27 JANUARY 2021 AT 6.30 PM

IN THE COUNCIL CHAMBER, SALISBURY COMMUNITY HUB, 34 CHURCH STREET, SALISBURY

MEMBERS

Mr T Mosel (Presiding Member)
Mr R Bateup
Ms C Gill
Mr B Brug
Mr M Atkinson

REQUIRED STAFF

General Manager City Development, Mr T Sutcliffe
Manager Development Services, Mr C Zafirooulos (Assessment Manager)
Senior Development Officer, Mr C Carrey

APOLOGIES

LEAVE OF ABSENCE

PRESENTATION OF MINUTES

Copy of the Endorsed Minutes of the Council Assessment Panel Meeting held on 22 December 2020.

DECLARATIONS OF CONFLICTS OF INTEREST

REPORTS

Development Applications

8.1.1 **361/778/2020/NB** **7**
859-885 Port Wakefield Road, Bolivar SA 5110
24 Hour Fast Food Restaurant With Associated Drive Through, Signage
(Including One (1) Twelve (12) Metre High Freestanding Illuminated Pylon
Sign), Carparking, Service Areas And Landscaping (Non-Complying)

OTHER BUSINESS

- 8.2.1 Status of Current Appeal Matters and Deferred Items
- 8.2.2 Policy Issues is Arising from Consideration of Development Applications
- 8.2.3 Future Meetings & Agenda Items

CLOSE

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**MINUTES OF COUNCIL ASSESSMENT PANEL MEETING HELD IN LITTLE PARA
CONFERENCE ROOMS, 34 CHURCH STREET, SALISBURY ON**

22 DECEMBER 2020

MEMBERS PRESENT

Mr T Mosel (Presiding Member)
Mr R Bateup
Ms C Gill
Mr B Brug
Mr M Atkinson

STAFF

General Manager City Development, Mr T Sutcliffe
Manager Development Services, Mr C Zafirooulos (Assessment Manager)
Team Leader Planning, Mr A Curtis
Team Leader Business Services, Mrs H Crossley

The meeting commenced at 6.24 pm.

The Presiding Member welcomed the members, staff and the gallery to the meeting.

APOLOGIES

Nil

LEAVE OF ABSENCE

Nil

ENDORSED MINUTES FROM PREVIOUS MEETING

The Minutes of the Council Assessment Panel Meeting held on 24 November 2020, be taken as read and confirmed.

DECLARATIONS OF CONFLICTS OF INTEREST

Nil

REPORTS

Nil

OTHER BUSINESS

8.2.3 Policy for the Assessment Panel Review of Decisions of the Assessment Manager

Mr R Bateup moved, and the Council Assessment Panel resolved that:

1. That the Council Assessment Panel resolves to adopt the *Policy for Assessment Panel Review of Decision of Assessment Manager* in Attachment 1.

8.2.2 Delegations

Ms C Gill moved, and the Council Assessment Panel resolved that:

1. In exercise of the power contained in Section 100 of the *Planning, Development and Infrastructure Act 2016* the powers and functions under the *Planning, Development and Infrastructure Act 2016* and regulations statutory instruments made thereunder contained in the proposed Instrument of Delegation as Attachment 1 to this report (Council Assessment Panel, 22 December 2020, Item No. 8.2.2) are hereby delegated, effective from the day on which the Council's Development Plan is revoked by the Minister by notice in the Gazette pursuant to Clause 9(7) of Schedule 8 of the *Planning, Development and Infrastructure Act 2016*, to the positions identified in the third column of the Instrument of Delegation subject to the conditions and/or limitations, if any, specified herein or in the Schedule of Conditions in the proposed Instrument of Delegation.
2. The delegated powers and functions may be exercised individually by each delegate in respect of any particular matter where the delegate is required or proposing to act in the course of their duties.
3. Such powers and functions may be further delegated by the Assessment Manager in accordance with Section 100(2)(c) of the *Planning, Development and Infrastructure Act 2016* as the Assessment Manager sees fit, unless otherwise indicated herein or in the Schedule of Conditions contained in the proposed Instrument of Delegation.
4. The power in Sections 119(9) and (14) of the *Planning, Development and Infrastructure Act 2016* and Regulation 7 of the *Planning, Development and Infrastructure (Fees, Charges and Contributions) Regulations 2019* may be further delegated by the General Manager City Development in accordance with Section 100(2)(c) of the *Planning, Development and Infrastructure Act 2016* as the General Manager City Development sees fit, unless otherwise indicated in the

Schedule of Conditions contained in the proposed Instrument of Delegation.

5. That the delegations of the Panel be reviewed within 12 months of the day on which the Council's Development Plan is revoked by the Minister by notice in the Gazette pursuant to Clause 9(7) of Schedule 8 of the Planning, Development and Infrastructure Act 2016.

8.2.1 Council Assessment Panel Operating Procedures

Mr M Atkinson moved, and the Council Assessment Panel resolved that:

1. That the City of Salisbury Council Assessment Panel General Operating Procedures, forming Attachment 1 to the agenda report, be adopted subject to the following amendments:
 - a. That clause 2.3 of the City of Salisbury Council Assessment Panel General Operating Procedures be amended to separate the new wording commencing from "Where attendance at the meeting..." to make a new clause.
 - b. That clause 3.6 of the City of Salisbury Council Assessment Panel General Operating Procedures be amended to delete the words in brackets.
 - c. That clause 6.13 of the City of Salisbury Council Assessment Panel General Operating Procedures be amended after the words 'development application' to include the following; 'under the Development Act 1993 or an application for which notice must be given under the Planning Development and Infrastructure Act 2016.'

8.2.4 Council Assessment Panel 2021 Meeting Schedule

Mr R Bateup moved, and the Council Assessment Panel resolved that:

1. That the Council Assessment Panel 2021 meeting schedule forming Attachment 1 to the agenda report, be adopted.

8.2.5 Status of Current Appeal Matters and Deferred Items

Nil

8.2.6 Policy Issues Arising from Consideration of Development Applications

Nil

8.2.7 Future Meetings & Agenda Items

Next meeting scheduled for Wednesday 27 January 2021.

ADOPTION OF MINUTES

Mr B Brug moved , and the Council Assessment Panel resolved that the Minutes of the Council Assessment Panel Meeting be taken and read as confirmed.

The meeting closed at 7.44 pm.

PRESIDING MEMBER:

Mr T Mosel

DATE:

22 December 2020

(refer to email approving minutes registered in Dataworks Document Number 6387518)

ITEM	8.1.1
	COUNCIL ASSESSMENT PANEL
DATE	27 January 2021
APPLICATION NO.	361/778/2020/NB
APPLICANT	McDonalds Australia Ltd
PROPOSAL	24 Hour Fast Food Restaurant With Associated Drive Through, Signage (Including One (1) Twelve (12) Metre High Freestanding Illuminated Pylon Sign), Carparking, Service Areas And Landscaping (Non-Complying)
LOCATION	859-885 Port Wakefield Road, Bolivar SA 5110
CERTIFICATE OF TITLE	CT-6169/78
AUTHOR	Tom Gregory – Planning Consultant

1. DEVELOPMENT APPLICATION DETAILS

Zone/Policy Area	Primary Production Zone Open Space Zone – Recreation Policy Area 15 as depicted on Maps Sal/23 and Sal/31
Application Type	Non-Complying
Public Notification	Representations received: No representations received
Referrals - Statutory	DIT – Department for Infrastructure and Transport (Commissioner of Highways) Epic Energy (informal)
Referrals – Internal	Urban Policy and Planning Environmental Health Parks & Open Space Assets Development Engineering
Development Plan Version	Salisbury (City) Development Plan Consolidated 4 April 2019
Assessing Officer	Tom Gregory – Planning Consultant
Recommendation	Grant Development Plan Consent subject to conditions
Meeting Date	27 January 2021

2. REPORT CONTENTS

Assessment Report

Attachment 1:	Proposal Plans and Supporting Documentation including Statement of Support and Statement of Effect
Attachment 2:	Notice of Category 3 Development
Attachment 3:	Schedule 8 Referral Response
Attachment 4:	Extract of Relevant Development Plan Provisions and Location Maps (Consolidated 4 April 2019)

3. EXECUTIVE SUMMARY

McDonald's Australia Limited is seeking Development Plan Consent for a restaurant with integrated drive through, car parking, landscaping and associated signage at the southern end of 859-885 Port Wakefield Road, Bolivar.

The proposed development has been assessed as a 'non-complying' form of development and Category 3 for the purposes of public notification. No responses were received during the public notification period.

This report provides a detailed assessment of the application against the relevant provisions of the Salisbury Development Plan. The assessment found that:

- (a) The proposed development is an appropriate use of the land that will complement other similar retail offerings that exist and are operating on the subject land.
- (b) The development can be wholly accommodated on the subject land.
- (c) It is highly unlikely that the subject site will be contemplated for rural farming type activities in accordance with the objectives of the Primary Production zoning of the land. This is further reinforced by the recent development of the OTR service station and associated retail development on the northern section of the subject land, assessed at the time as a merit-assessed non-defined land use.
- (d) The incorporation of part of the land into the Open Space Zone is unusual; however the elements of the development located within it are relatively low in scale and predominantly comprise a driveway, landscaping and open style fencing.
- (e) Despite the Primary Production zoning of most of the land, the OTR has been developed on the majority of the subject land which has a strong bearing on the future use of the vacant site.
- (f) The proposed development has been designed and sited having regard to the nature and scale of the adjoining OTR development and will increase the range of current retail offerings for what is considered a highway 'service centre'.
- (g) The subject land is removed from nearby residential land meaning the proposed use could operate without adverse impacts on sensitive receptors.
- (h) The overall height, bulk and scale of the proposed restaurant building is compatible with the existing scale and form of the existing commercial buildings in the locality, and consistent with the emerging commercial character.
- (i) Despite the Development Plan encouraging an aggregation of shopping and convenience facilities within Centre Zones, the assessment determined that the subject land is appropriate for the proposed use and it is suggested that the use will not have a detrimental effect on any nearby centre.
- (j) Sufficient car parking can be provided on the site for the proposed development.
- (k) Access and egress to/from the site will be via existing approved crossovers and driveways.
- (l) The 24-hour 7 day a week operation of the restaurant is considered appropriate given it is consistent with the other commercial operations on the land, and the land is located away from sensitive receptors.

Please note: concurrence from the State Commission Assessment Panel is not required for non-complying development applications following the 14 May 2020 passing of the *Covid-19 Emergency Response (Further Measures) Amendment Act 2020* by both Houses of Parliament.

Given the above, and despite being a non-complying form of development it is recommended that Development Plan Consent be granted, subject to Conditions of Approval.

4. SUBJECT SITE

The subject land comprises a single parcel: 859-885 Port Wakefield Road, Bolivar, formally described as Allotment 25 in Deposited Plan 85405, Certificate of Title Volume: 6169 Folio 78.

The allotment retains an area of 5.4 hectares with a frontage to Hodgson Road of approximately 189 metres to the north, and Port Wakefield Road of approximately 251 metres to the east.

The land is situated at the intersection of Port Wakefield Road and Hodgson Road, Bolivar. Hodgson Road is a link road between Port Wakefield Road and the Northern Connector which runs alongside of the subject land to the west. Hodgson Road provides access to the OTR development which is located on the northern side of the land and occupies approximately two thirds of the allotment.

The OTR is an integrated petrol station and service/convenience centre incorporating fuel sales; car/dog washing facilities; fast food, café and bakery chains, a mini-supermarket; and other convenience/retail services and operates 24 hours per day, 7 days per week. The OTR is referred to as a highway ‘service centre’ for the purposes of this report.

The OTR is comprised of several commercially designed buildings together with several canopies surrounded by extensive hardstand areas, freestanding signage and associated landscaping.

The subject site is located within the south east portion of the subject land and is currently undeveloped. The site has a frontage to Port Wakefield Road of approximately 51 metres.

The topography of the site is relatively flat; however a gentle rise/mounding is evident towards the west. There is no indication of a future intent for the use of the land to the west.

The site is devoid of significant features apart from a number of sporadically spaced trees, some of which are intended to be removed. Of the trees to be removed it has been established that they are not of a size to be of Regulated or Significant status, nor are they protected by the *Native Vegetation Act 1991*.

The land contains two easements: the largest of which runs along the Port Wakefield Road frontage in favor of the Natural Gas Authority of SA – Epic Energy; and the smaller is far removed from the subject site and abuts a section of the Hodgson Road frontage. The larger easement is identified on the site plan as abutting the subject site but not being impacted by the proposed development.

There are no Land Management Agreements or encumbrances registered on the Certificate of Title.

Site photos are provided on the following page.

Photo 1.
Within the subject site, looking east towards Port Wakefield Road, across the development site



Photo 2.
Within the subject site, looking east towards the existing point of access from Port Wakefield Road



Photo 3.
*Panorama
 View looking
 west - with
 the
 development
 site indicated
 by the white
 star
 (Source:
 NearMap)*



5. LOCALITY



The locality is principally defined by visual reference.

Dominant features in the locality include the OTR ‘service centre’ and the White Horse Inn Hotel to the north. Port Wakefield Road and the Northern Connector also have a commanding presence in the locality which contrasts with the surrounding open space areas that include the Little Para River corridor and wetlands to the south and east. Further afield and visually separated by the open space and vegetated corridors are the western sections of the suburbs of Parafield Gardens and Paralowie. Whilst the residential nature of the suburbs is noted they are sufficiently removed from the primarily commercial and open space nature of the locality.

A locality plan and contextual plan are provided below.

Locality Plan - Aerial





Legend (Source: Esri)	
	Site boundary
	Locality boundary

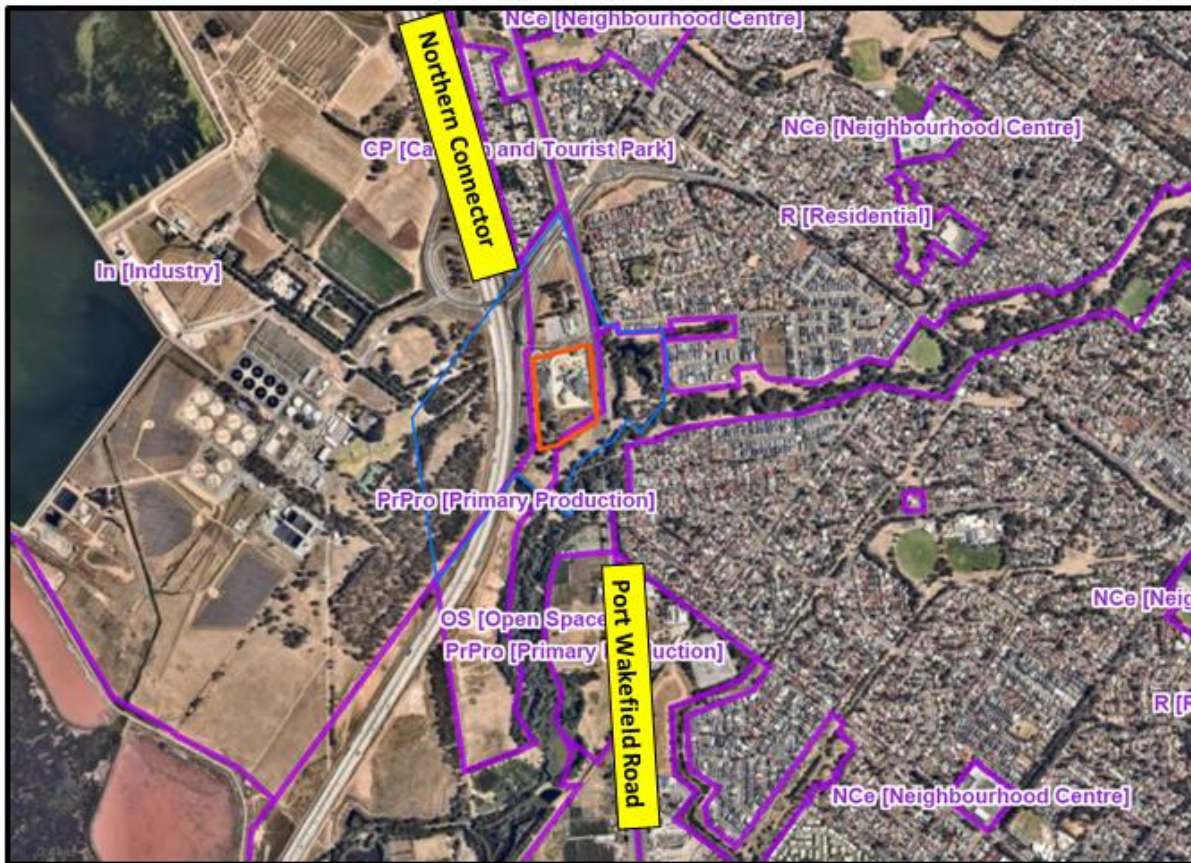
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Locality Plan – Cadastre



Legend (Source: Esri)	
	Site boundary
	Locality boundary

Contextual Plan



Legend (Source: Esri)	
	Site boundary
	Locality boundary
	Zone boundary

6. DESCRIPTION OF THE PROPOSED DEVELOPMENT

The proposed development comprises the construction of a fast-food restaurant/café/take away together with an integrated drive through, car parking areas, landscaping and signage.

Trading hours are proposed to be 24 hours per day, 7 days per week.

The proposal includes a principal building surrounded by the ancillary components. The building has a floor area of 400m² and accommodates the following:

- dining room;
- café and serving facilities;
- two serveries to serve patrons of the drive through;
- kitchen and ‘back of house’ storage;

- staff facilities and amenities;
- patron amenities;
- service area (corral); and
- small play area for children.

The building is setback some 36 metres from Port Wakefield Road.

The building adopts a simple form comprising a 6.057 metre high parapet which screens the roof and roof mounted plant, with an elevated glazed feature corner rising 5 metres which accommodates the play facilities in the play area and allows access to natural light into this aspect of the building.

Articulation is provided to the building through the use of a variety of building materials and colours. Vertical and horizontal projections and the use of shade canopies and window placement / building openings highlight various aspects of the land use, such as defining service points, building entries and windows.

Signage is also incorporated as a design feature and is coordinated throughout the development both being affixed to the building and freestanding. The proposed signage includes:

- A freestanding 12 metre high pylon sign located adjacent to the Port Wakefield Road adjacent the entry to the land. The sign is proposed to be internally illuminated and will feature corporate signage (McDonald's 'Big M' and 'McCafe' and an information sign advising of the 24/7 nature of the operations); and
- Ancillary signage includes wall signs, directional signs, menu board signs, speed signs, and pedestrian access signs.

Access to the development will be via the existing crossovers that service the OTR which will provide entry into the one-way drive through lane and an entry/exit to the main car park. Exiting vehicles would traverse back through the OTR site and then onto the adjoining road network. All vehicles will enter and exit the site in a forward direction.

Off-street parking is provided for 39 vehicles, including two stacked staff carparks, one disabled car park and two waiting bays. The access driveways and parking areas are designed and proposed to be constructed to meet the current AS 2890.1.

A loading bay is located adjacent to the southern elevation of the building and is designed to allow a heavy rigged vehicle up to 11.6 metres in length to enter and exit the site in a forward direction following several onsite manoeuvres.

Full-time equivalent staffing numbers are not specified; however the applicant has advised that McDonald's restaurants of the size proposed can provide employment opportunities for up to 100 persons on a full and part time basis, particularly for younger people within the surrounding area.

A detailed landscaping plan has been provided which includes a combination of low and medium level species, ground covers, grasses and larger tree species. Mature stock will be planted for the feature trees, and the Applicant proposes to establish an ongoing maintenance regime.

The existing rural post and wire fencing is proposed to be maintained to the Port Wakefield Road frontage and to the southern property boundary, with the remainder of the site otherwise remaining open.

Commercial waste management standards will apply for the facility with two commercial bins located in the corral area. Staff will be responsible for keeping the site clean and free of litter.

A copy of the proposal plans and supporting documentation are contained in Attachment 1.

7. CLASSIFICATION

The subject land is primarily located within the Primary Production Zone as depicted on Map Sal/31 of the Salisbury Council Development Plan, consolidated 4 April 2019.

It is noted that the southern portion of the land is also located within the Open Space Zone, Recreation Policy Area 15 which on face value is unusual, as in normal circumstances zone boundaries follow cadastral lines, yet in this instance the zone boundary has been clearly identified for reasons presently unknown.

The Primary Production and Open Space Zones provide a definitive list of various types of development that are ‘non-complying’ for assessment purposes, in some cases subject to exceptions:

Primary Production Zone

A shop or group of shops is listed as a non-complying form of development within the Primary Production Zone except where it is located outside of the Aircraft Noise Policy Area 16 and Horticulture Policy Area 17 and it achieves all of the following:

- a) it is ancillary to and in association with a petrol filling station or a primary production or tourism development*
- b) it has a maximum gross leasable area of no more than 150 square metres.*

Advertisement and/or advertising hoarding is listed as a non-complying form of development within the Primary Production Zone where the development achieves at least one of (a) or (b):

- a) is adjacent to a road with a speed limit of less than 80 km/h*
- b) has an advertisement area of 2 square metres or less and achieves all of the following:*
 - i. the message contained thereon relates entirely to a lawful use of land*
 - ii. the advertisement is erected on the same allotment as the use it seeks to advertise*
 - iii. the advertisement will not result in more than two advertisements on the allotment.*

Open Space Zone

A shop or group of shops is listed as a non-complying form of development within the Open Space Zone.

Further, a ‘shop’ is a defined land use within Schedule 1 of the *Development Regulations 2008* and means:

Shop means—

- a) premises used primarily for the sale by retail, rental or display of goods, foodstuffs, merchandise or materials; or
- b) a restaurant; or
- c) a bulky goods outlet or a retail showroom; or
- d) a personal service establishment

but does not include—

- e) a hotel; or
- f) a motor repair station; or
- g) a petrol filling station; or
- h) a plant nursery where there is no sale by retail; or
- i) a timber yard; or
- j) service trade premises; or
- k) service industry.

The proposed use does not satisfy the exceptions above and is therefore a non-complying form of development for assessment purposes.

Other elements including the drive-through, car parking, and landscaping are ancillary and incidental to the establishment of the restaurant (shop).

Section 35(3) of the *Development Act 1993* that required the concurrence of the State Commission Assessment Panel for non-complying development has been deleted. The Panel is therefore the relevant authority in its own right and is not required to seek the concurrence of the Commission if it determines to grant Development Plan Consent.

8. PUBLIC NOTIFICATION

The proposed development is neither assigned to Category 1 or Category 2 within the Primary Production or Open Space Zones of the Development Plan or Schedule 9 of the *Development Regulations 2008*. On this basis, the application is a Category 3 form of development for the purposes of public notification.

The application was notified as a Category 3 development between 13 November and 26 November 2020, with a notice of the application published in The Advertiser newspaper and notices issued to owners/occupiers of adjoining land and those within 60 metres of the subject site.

Council did not receive any representations during the public notification period.

A copy of the Category 3 public notice is contained in Attachment 2.

9. REFERRALS – STATUTORY

The application was subject to the following statutory referral in accordance with Schedule 8 (3) of the *Development Regulations 2008*.

Agency	Comment
<p>Commissioner of Highways Department of Infrastructure and Transport (DIT)</p>	<p>On the basis that the proposed development would change the anticipated volume of vehicles utilising the existing access points to/from Port Wakefield Road, an identified Primary Arterial Road, the application was referred to the Department of Infrastructure and Transport - Commissioner of Highways pursuant to Section 37 of the <i>Development Act 1993</i> and Schedule 8 of the <i>Development Regulations 2008</i>.</p> <p>The advice from DIT did not object to the proposed development however it sought additional information in relation to traffic generation and distribution in relation to the site and surrounding road network. A response was provided from GTA Consultants to address the matters raised by DIT, a copy of which is included in the Attachments.</p> <p>DIT provided a further response noting support for the proposal and confirming its authorisation to utilise the existing access point on Port Wakefield Road to service the site.</p> <p>DIT supports the access arrangements and provided a series of conditions relating to vehicle access and stormwater run-off to be attached to any consent issued. These conditions are included in the below recommendation.</p>
<p>Epic Energy</p>	<p>Given the proximity of the easement running along Port Wakefield Road and the Development Constraints Map Sal/31 of the Development Plan identifying the location of an Epic Energy Gas Pipeline, an informal referral was sent to Epic Energy for comment.</p> <p>Epic Energy provided a response stating it has no objection to the proposed development given no services are proposed to cross the pipeline easement and no structure is proposed on the easement. Epic Energy stated it would like to review and approve the detailed design once available.</p> <p>The applicant provided a response to this and reaffirmed there will be no services required to cross the easement as they will all come from inside the site.</p> <p>It is noted that there is no legislative mechanism to ensure Epic Energy are provided with a copy of the detailed design for review and comment. Should the applicant choose to further consult with Epic Energy in this regard, it can do so out of 'good faith'.</p>

On the information provided it can be reasonably determined that the proposal will not impact the Epic Energy easement or infrastructure contained within it.

10. REFERRALS – INTERNAL

Department	Comment
<i>Development Engineering</i>	<p>Council’s Development Engineer reviewed the application and sought additional information from the applicant relating to vehicle turn paths, site works and drainage.</p> <p>The Applicant provided an amended documentation including a drainage plan; overland flow diversion area sketch; MUSIC and DRAINS model outputs. The Division is satisfied that the revised plans and information adequately address the initial concerns, and provided the site is developed in accordance with the amended documentation the Development Engineer has no further comment.</p> <p>The below recommendation includes a condition of consent ensure the development (if approved) will proceed in accordance with the latest updated information.</p>
<i>Urban Policy and Planning</i>	<p>Council’s Urban Policy and Planning Division identified that there are no strategic issues in relation to the proposal on the subject site. The Division noted that the proposal results in an acceptable combination of uses that are already happening in many locations in this locality and would become a service centre activity in a location that seems suitable to both Port Wakefield Road and the Northern Connector.</p> <p>The Division noted the ‘minor intrusion’ of the Open Space Zone into the subject land and suspects the zone boundary was meant to align with the property boundary in that location. As stated elsewhere in this report, nothing turns on this matter procedurally, however the below assessment will need to have some regard to the zone’s existence.</p>
<i>Environmental Health</i>	<p>Council’s Environmental Health Division raised no objection to the proposed development and suggested an advisory note be placed on any consent confirming that the design of food premises must be in compliance with the Food Act 2001 and the relevant Food Safety Standard. An advisory note to this effect is included in the recommendation accompanying this report.</p>
<i>Parks and Open Space Assets</i>	<p>Council’s Parks & Open Space Assets Division reviewed the proposed development and provided constructive comments on the proposed landscaping plan which resulted in a number of minor amendments and an updated species list. The Division has reviewed the revised plans and is</p>

	satisfied that the comments have been addressed.
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11. ASSESSMENT

Pursuant to Section 35(2) of the *Development Act 1993*, it is recommended that the Panel determine that the proposed development is not seriously at variance with the Salisbury Development Plan, Consolidated 4 April 2019. The following reasons are given in support of this recommendation:

- a) The subject land has a long history of non-primary production use, and given the constraints and other commercial uses of the land, it is unlikely that the land will be contemplated for primary production uses into the future.
- b) The proposed development comprises an expansion to an established commercial land use with consistent hours of operation.
- c) Whilst commercial in nature, the scale, mass and built form of the proposed development will not detract from the character or amenity of the locality through appropriate setbacks, design and landscaping.
- d) The proposed development retains a sufficient separation distance from the closest residential properties / sensitive receptors so as not to give rise to amenity impacts as a result of the 24/7 hours of operation proposed.
- e) Despite the zone boundary anomaly, the elements of the development within the Open Space Zone are relatively low in scale, small in mass and predominantly comprised of a driveway and landscaping area and open style fencing which maintain an outlook to the Little Parra River corridor.

Assessment

A detailed assessment of the application has taken place against the relevant provisions of the Salisbury Development Plan and is described below under relevant headings.

An extract of the relevant Development Plan, Consolidated 4 April 2019, is contained in Attachment 4. The relevant provisions are also highlighted in the Attachment.

Land Use

The Objectives of the Primary Production Zone seek the long-term continuation of primary production; economically productive, efficient and environmentally sustainable production; and the protection of such land from encroachment of incompatible land uses.

The Objectives of the Open Space Zone seek to preserve the open space character to provide a visual contrast to the urban area; and provide for a range of passive and active recreation land uses.

It is understood that the subject land has had a long history of non-primary production use. The land is bound by roads on three sides and the Little Para River corridor to the south and it is considered unlikely that it would be used for primary production purposes into the future. Approximately two thirds of the land is currently occupied by the OTR development, with the subject site forming part of the undeveloped southern portion of the land. It is considered highly unlikely that it would ever be contemplated that the site would ever be used for development other than those involving commercial land uses, and it certainly has no future for rural uses in accordance with the current zoning given its site / locality characteristics.

From a planning policy perspective, Development Plans traditionally establish a hierarchy of integrated centres as the focus for shopping, administrative, cultural, community, entertainment, educational, religious and recreational facilities. Centres are encouraged to be developed in accordance with a hierarchy based on function, so that each type of centre provides a portion of the total requirement of goods and services commensurate within its role.

Commercial development is primarily encouraged within a centre zone, yet there are numerous examples within the City of Salisbury and elsewhere where commercial development has occurred outside of a centre zone, such as the OTR development on the subject land. In instances such as these where commercial development is proposed outside of a centre zone, consideration must be given to determine whether the commercial use is appropriate, and whether or not the use can function without having a detrimental impact on an existing nearby centre.

In the documentation provided by the Applicant it has been stated that fast food restaurants such as McDonald's are a destination in their own right and do not form part of the day to day shopping needs of the community that necessitate a Centre Zone location; and that these types of restaurants are traditionally sited on arterial roads from which they draw a large percentage of their custom – on average some 60% of custom is generated by the drive-through facility, whilst the vast majority of customers attend the stores by car rather than by foot or public transport.

The applicant also suggests that the customer base is different in that the reason to visit a fast-food restaurant differs from the day-to-day shopping trip normally associated with the reason to visit a shopping centre.

Be that as it may, it would appear that the proposed development is an expansion of an existing commercial site rather than a new intrusion of commercial development in this location given it is evident that the development has been designed in relationship with the adjoining commercial development and its relationship to the road network.

In considering nearby centre zones it is noted that there are four (4) within a 1km radius of the subject land, the majority of which are surrounded by residential areas with access to lower order arterial or collector roads, which we understand is of importance to the Applicant.

The closest Neighbourhood Zone to the north (approximately 1km) adjoins Port Wakefield Road. This centre contains a large public reserve to the west of the main retail precinct (Paralowie Village Shopping Centre) which notably contains a number of existing fast-food restaurants (including Hungry Jacks, KFC, and McDonald's).

The Development Plan test for commercial development out of the Centre is considered twofold. Firstly, whether the proposed use will detract from other established uses to a degree that the centre zone becomes unviable, and second, whether the use can occur on the subject land without adverse impacts on the amenity and/or without jeopardizing the attainment of the objectives of the relevant zone.

The following is considered to be relevant:

1. A retail/commercial analysis has not been provided, however it is suggested that the proposed development will not detract from other established uses to a degree that a nearby centre becomes unviable. It is evident however through the development of the greater OTR 'service centre' that the target clientele is primarily for the driver and/or traveller; with the secondary clientele market for those persons seeking out the 'destination in its own right'. The proposed development can be seen as an expansion of an existing commercial site rather than a new intrusion of commercial development in an out-of-centre location. This is further evidenced by the design of the restaurant being in such a manner that relates to the adjoining commercial development.
2. For the reasons outlined herein, the proposed development can occur without having an adverse impact on the amenity of the locality, and despite the zoning of the land it is unlikely to be contemplated for development other than those involving commercial land uses, and it certainly appears unviable for future rural uses in accordance with the current zoning.

Whilst the proposed land use is not in accordance with the subject zoning of the land in the Development Plan, the proposed development is considered to be appropriate when taking into consideration the viability of the land for rural purposes and the existing approved commercial uses on site.

Land Use Conflict

The Development seeks to ensure that particular uses of land are located, and or designed in such a way, to minimise impacts on each other as per Principle of Development Control 1 of the 'Interface Between Land Uses' state:

- 1 *Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:*
 - (a) *the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants;*
 - (b) *noise;*
 - (c) *vibration;*
 - (d) *electrical interference;*
 - (e) *light spill;*
 - (f) *glare;*
 - (g) *hours of operation;*
 - (h) *traffic impacts.*

The locality portrays a moderate level of commercial amenity on the balance of the subject land, and it is noted that these operations are also 24 hours a day, 7 days a week.

There is also significant separation between the subject land and the nearest sensitive receptors. The nearest sensitive receptors are also separated by Primary Arterial roads which there-in further reduces the potential for impacts on amenity.

Design and Appearance

The following general provisions of the Development Plan, contained under the ‘Design and Appearance’ module are considered to be relevant to the design and appearance of the proposed development.

1. *The design of a building may be of a contemporary nature and exhibit an innovative style provided the overall form is sympathetic to the scale of development in the locality and the context of its setting with regard to shape, size, materials and colours.*
2. *Buildings should be designed and sited to avoid creating extensive areas of uninterrupted walling facing areas exposed to public view.*
3. *Buildings should be designed to reduce their visual bulk and provide visual interest through design elements such as:*
 - a) *Articulation;*
 - b) *colour and detailing;*
 - c) *small vertical and horizontal components;*
 - d) *design and placing of windows; and*
 - e) *variations to facades.*
14. *Development should be designed and sited so that outdoor storage, loading and service areas are screened from public view by an appropriate combination of built form, solid fencing and/or landscaping.*
22. *The setback of buildings from public roads should:*
 - a) *be similar to, or compatible with, setbacks of building on adjoining land and other buildings in the locality;*
 - b) *contribute positively to the streetscape character of the locality; and*
 - c) *not result in or contribute to a detrimental impact upon the function appearance or character of the locality.*

The proposed building is of a contemporary commercial design that is considered to be both functional and visually appealing. The scale, mass and built form is complementary to and consistent with other buildings within the locality. The use of awnings, openings and windows will break up the form and provide visual interest.

The building is to be located within an area that has a dominant commercial character deriving from the range of uses aligned along Port Wakefield Road where buildings are generally commercial/industrial in nature, scale, appearance and form.

Given the nature and location of the drive-through, all facades and areas of the building will be visible by the public. The provision of uninterrupted walling has been reduced, and the storage and loading areas have been appropriately integrated into the design as opposed to appearing like an ‘after thought’.

The use of materials is appropriate, and the generous setback from Port Wakefield Road will result in a visually appropriate development.

The building is surrounded by hardstand and paved surfaces for car parking and vehicle manoeuvring, however the landscaping plan and scheduled ongoing maintenance of the vegetation will ensure a visually attractive and softer outcome. The landscaping proposes an array of low level grasses, ground covers, and shrubs together with larger mature plantings. It is acknowledged that the larger plants are proposed on the western side of the building which will appear as a landscaped backdrop from the road. This is also considered appropriate given the relationship to and proximity of the Open Space Zone and adjoining corridor.

In summary, the overall design and appearance of the development is considered to be of an appropriate standard for this type of development and will not detract from the current commercial character of the locality.

Advertisements

The following Principles of Development Control are considered to be relevant to the proposed development in respect to advertising:

1. *The location, siting, design, materials, size and shape of advertisements and/or advertising hoardings should be:*
 - a) *consistent with the predominant character of the urban or rural landscape;*
 - b) *in harmony with any buildings or sites of historic significance or heritage value in the area; and*
 - c) *co-ordinated with and complement the architectural form and design of the building they are to be located on.*

15. *Freestanding advertisements and/or advertising hoardings should be:*
 - a) *limited to only one primary advertisement per site or complex; and*
 - b) *of a scale and size in keeping with the desired character of the locality and compatible with the development on the site.*

18. *Freestanding advertisements should not exceed the total height (measured from natural ground level) as specified within the following table:*

In all other locations: 3 metres

The proposed signage will incorporate the 'McDonald's' branding that is typical of the restaurants across Australia. The signage is considered to be of scale, size and frequency appropriate for a commercial development and compatible with other signage on the subject land.

It should be noted however that the signage is at odds with that typically envisaged within the Primary Production and Open Space Zones.

The applicant submits that the 12 metre high freestanding sign is consistent with McDonald's sites nationally. It further submits that along the extensive frontage to Port Wakefield Road a single freestanding sign exists, and the placement of one additional freestanding sign with identical height to the existing OTR development is such that sufficient space is provided

between the signs to avoid what might be described as signage clutter. In fact, the proposed freestanding sign is separated from the existing pylon sign by approximately 40 metres which means both pylon signs will be relatively clustered and moreover concentrated on the southern portion of the land in close proximity to the main point of entry.

The pylon sign itself is an element that is reasonably contemplated with such a land use and although it is a tall structure, the pylon sign is considered appropriate on the basis it is set back from Port Wakefield Road, which in this location is a 5-lane primary arterial road with a separated grassed median. Furthermore, visibility of the pylon sign from the south looking north will be partially obscured by trees until a driver has crossed the bridge of the Little Para River. When heading south towards Adelaide from the north, visibility of the sign would be consistent with the existing OTR pylon sign, *Krispy Kreme* sign and stobie poles. The trees lining the river bank would also provide a softening backdrop.

As such, whilst not contemplated at the zone level, the proposed pylon sign is considered to be consistent with other similar signs in this part of the commercial precinct and will not have a detrimental impact on the character of the locality.

The other elements of the proposed signage (predominantly on the building) are not considered to be of an excessive scale, nor are they of a nature that will distract drivers. Signage on and around the curtilage of the building appears consistent with other similar restaurants.

The proposed signage does not move, rotate (other than the menu board which is changed at different meal times during the day/night) or incorporate flashing lights. The applicant states that the lighting lux is not so high as to impact the safety of passing traffic. It is proposed to reinforce this by way of condition should the application be approved.

Although signage is generally not envisaged at the zone level, the extent of signage proposed is consistent with the prevailing character of commercial signage elsewhere on the subject land.

DIT has recommended that the all signage is consistent with its publication '*Advertising Signs: Assessment Guidelines for Road Safety*' which is considered appropriate given the location of the subject land in relation to Port Wakefield Road. A condition to this effect is outlined in the below recommendation.

Overall, the signage and advertising elements of the proposal are considered to be appropriate and generally satisfy the relevant provisions of the Development Plan.

Car Parking, Traffic and Access

Access to the site is via existing crossovers and internal roads that presently service the OTR service centre. The Applicant has advised that wayfinding signs will be installed at key locations within the subject land to ensure appropriate access to and from the proposed development.

A total of 39 off-street car parks are provided, including two stacked staff car parks, one disabled persons car park and two waiting bays. All access ways and parking will be designed

and constructed to meet current Australian Standards and therefore satisfy the relevant Development Plan policies.

In terms of off-street vehicle parking requirements Table Sal/2 of the Development Plan anticipates the provision of:

- 7 spaces per 100m² of gross leasable area for 'shops' located outside of centre zones; and
- Greater of 1 space for every 3 seats or 1 space for every 15 square metres dining area for 'restaurants'.

No matter whether the proposal is classified as a 'shop' or 'restaurant' for these purposes, the application results in a minimum 'surplus' of 11 off-street vehicle parking spaces. In addition, given the anticipated 60% of customers using the drive-through facility, the extent of car parking on site is considered appropriate and consistent with the relevant Development Plan policies.

In relation to traffic and access, the Applicant engaged GTA Consultants to provide a response to the comments raised by DIT. This report confirms that the proposed development will have a minor impact on the adjacent road network, intersections and access points for the OTR site. It is approximated that the use will generate 150 vehicles per hour at peak use and approximately 2,000 per day, with a high level of passing trade to be expected.

The traffic assessment confirms there is available capacity in the adjacent road network for the proposed development, even assuming no passing trade discounts. The total daily and peak hour traffic generation of the site is considered in the assessment to be 'minor' and well within the spacer capacity given the existing traffic volumes on Port Wakefield Road, Northern Connector ramps and Hodgson Road.

This together with the supportive comments received from DIT and Council's Development Engineering division demonstrates that the proposal generally complies with the relevant Development Plan provisions in this regard, which effectively seek development that provides safe and efficient movement for all transport modes; adequate off-street parking; and development that is appropriately located so that it supports and makes beset use of existing transport facilities and networks (Objective 2 – Transportation and Access).

Waste Management

The applicant has not provided a waste management report, however included with the application is a statement that waste will be managed in accordance with industry standards and the proposed plans indicate provision of two (2) commercial bins located in the corral area.

The timing and frequency of the bin collection is not presently known, however given the site is far removed from sensitive noise receptors the finer details are not considered to be an overly critical element in determining the merits of the proposal.

There is a level of information provided by the applicant regarding waste which generally satisfies the relevant provisions of the Development Plan. It is recommended that this further be reinforced by way of condition of approval.

Fencing and Landscaping

The Development Plan encourages development to be enhanced with appropriate planting using locally indigenous plant species where possible. It also envisages functional fences and walls that enhance the attractiveness of development. (Objectives 1 and 2 – Landscaping, Fences and Walls).

The proposal includes a landscaping plan which includes a combination of low and medium level species, ground covers, grasses and larger tree species. Mature stock will be planted for the feature trees, and the Applicant proposes to establish an ongoing maintenance regime.

The existing rural post and wire fencing is proposed to be maintained to the Port Wakefield Road frontage and to the southern property boundary, with the remainder of the site otherwise remaining open.

Council's Parks & Open Space Assets Division reviewed the proposed development and provided constructive comments on the proposed landscaping plan which resulted in a number of minor amendments and an updated species list.

The landscaping plan provided is considered appropriate in that it is consistent with the relevant Development Plan provisions. Further, it is noted that additional landscaping and open fencing is proposed within the portion of the site that is located within the Open Space Zone. This is considered to be appropriate and an attractive transition between the built-up area and the open space corridor.

The proposal is considered to generally satisfy the requirements of the Development Plan in this regard.

12. CONCLUSION

McDonald's Australia Limited is seeking Development Plan Consent for a restaurant with integrated drive through, car parking, landscaping and associated signage at the southern end of 859-885 Port Wakefield Road, Bolivar.

The proposed development has been assessed as a 'non-complying' form of development and Category 3 for the purposes of public notification. No responses were received during the public notification period.

This report provides a detailed assessment of the application against the relevant provisions of the Salisbury Development Plan. The assessment found that:

- (a) The proposed development is an appropriate use of the land that will complement other similar retail offerings that exist and are operating on the subject land.
- (b) The development can be wholly accommodated on the subject land.
- (c) It is highly unlikely that the subject site will be contemplated for rural farming type activities in accordance with the objectives of the Primary Production zoning of the land. This is further reinforced by the recent development of the OTR service station and associated retail development on the northern section of the subject land, assessed at the time as a merit-assessed non-defined land use.

- (d) The incorporation of part of the land into the Open Space Zone is unusual, however the elements of the development located within it are relatively low in scale and predominantly comprise a driveway, landscaping and open style fencing.
- (e) Despite the Primary Production zoning of most of the land, the OTR has been developed on the majority of the subject land which has a strong bearing on the future use of the vacant site.
- (f) The proposed development has been designed and sited having regard to the nature and scale of the adjoining OTR development and will increase the range of current retail offerings for what is considered a highway 'service centre'.
- (g) The subject land is removed from nearby residential land meaning the proposed use could operate without adverse impacts on sensitive receptors.
- (h) The overall height, bulk and scale of the proposed restaurant building is compatible with the existing scale and form of the existing commercial buildings in the locality, and consistent with the emerging commercial character.
- (i) Despite the Development Plan encouraging an aggregation of shopping and convenience facilities within Centre Zones, the assessment determined that the subject land is appropriate for the proposed use and it is suggested that the use will not have a detrimental effect on any nearby centre.
- (j) Sufficient car parking can be provided on the site for the proposed development.
- (k) Access and egress to/from the site will be via existing approved crossovers and driveways.
- (l) The 24-hour 7 day a week operation of the restaurant is considered appropriate given it is consistent with the other commercial operations on the land, and the land is located away from sensitive receptors.
- (m) The proposed development is not considered to be seriously at variance with the Development Plan and moreover is considered to generally comply with the relevant provision of the Development Plan to a sufficient degree.

Accordingly, it is recommended that Development Plan Consent be granted, subject to conditions.

13. STAFF RECOMMENDATION

That the Council Assessment Panel resolve that:

- A. The proposed development is not considered to be seriously at variance with the Salisbury Development Plan – Consolidated 4 April 2019.
- B. Pursuant to Section 33 of the *Development Act 1993*, Development Plan Consent is **GRANTED** to application number 361/778/2020/NB for 24 Hour Fast Food Restaurant With Associated Drive Through, Signage (Including One (1) Twelve (12) Metre High Freestanding Illuminated Pylon Sign), Carparking, Service Areas And Landscaping (Non-Complying) in accordance with the plans and details submitted with the application and subject to the following conditions:

Development Plan Consent Conditions

- 1. The development shall be carried out in accordance with the details submitted with the application and the following stamped approved plans and documents, except where otherwise varied by the conditions herein:

Drawing No.	Plan Type	Date	Prepared By
A001 Issue B	3D Views	26 October 2020	Richmond + Ross
A041 Issue A	Overall Site Plan	26 October 2020	Richmond + Ross
A062 Issue B	Site Plan	26 October 2020	Richmond + Ross
A067 Issue A	Car Swept Path	26 October 2020	Richmond + Ross
A068 Issue A	Truck Swept Path	26 October 2020	Richmond + Ross
A101 Issue B	Proposed Floor Plan	26 October 2020	Richmond + Ross
A201 Issue B	North & East Building Elevations	26 October 2020	Richmond + Ross
A202 Issue B	West & South Building Elevations	26 October 2020	Richmond + Ross
A205 Issue A	Finishes Schedule	26 October 2020	Richmond + Ross
A801 Issue B	Site Signage Plan	26 October 2020	Richmond + Ross
A806 Issue B	Signage Details	26 October 2020	Richmond + Ross
A807 Issue A	Signage Details	26 October 2020	Richmond + Ross
A802 Issue B	Pylon Sign Plan	26 October 2020	Richmond + Ross
A805 Issue A	Pylon Sign Details	26 October 2020	Richmond + Ross
22799/LA Revision A	Detailed Landscape Plan L01: Overall Layout	17 November 2020	Taylor's
22799/LA	Detailed Landscape Plan L02: Landscape Details	17 November 2020	Taylor's
C101 Issue B	Proposed Drainage Plan	7 December 2020	Richmond + Ross
C110 Issue A	Notes And Schedules	7 December 2020	Richmond + Ross
C301 Issue A	Sedimentation Plan	7 December 2020	Richmond + Ross
C310 Issue A	Erosion & Sedimentation Control Details	7 December 2020	Richmond + Ross
C350 Issue B	OSD Tank Plan And Section	7 December 2020	Richmond + Ross
C351 Issue A	Spell Filter Details	7 December 2020	Richmond + Ross
-	Email from Dean Nicolle (Currency Creek Arboretum) dated 13 May 2020	26 October 2020	Dean Nicholle
-	Statement In Support	26 May 2020	Access Planning
-	Statement Of Effect	26 October 2020	Access Planning
-	Response To Further Information Request	26 October 2020	Access Planning
S184940	Response To DIT RFI: Proposed McDonalds Restaurant Bolivar	15 December 2020	GTA Consultants

	(Adjacent OTR) – 15 December 2020		
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- * The approved documents referred to above are subject to change permitted by minor variations through the Building Rules Consent process as per Regulation 47A of the *Development Regulations 2008*.
- * Except where otherwise stated, the development shall be completed prior to occupation/commencement of use.

Reason: To ensure the proposal is established in accordance with the submitted plans.

2. Except where otherwise approved, no materials, goods or containers shall be stored in the designated car parking area or driveways at any time.

Reason: To ensure the car parking areas and manoeuvring areas are always available for the purpose they are designed. Further, that the site be maintained in a clean and tidy state.

3. All trade waste and other rubbish shall be contained and stored pending removal in covered containers which shall be contained within the building or otherwise screened from public view.

Reason: To maintain the amenity of the locality.

4. All mechanical services to the building and in conjunction with the proposed use shall be designed, installed and operated in such a manner that any person or persons working within or adjacent to the site should not be subjected to any nuisance or inconvenience from noise or fumes.

Reason: To limit the effect of the mechanical services for activities on the subject land within the site, thereby maintaining the amenity of the locality.

5. All loading and unloading of vehicles and manoeuvring of vehicles in connection with the now approved development shall be carried out entirely within the subject land.

Reason: To ensure that vehicles associated with the land use does not cause disruption or danger to vehicles on adjoining public roads.

6. All landscaping identified on the Landscape Plan approved by Council under Development Plan Condition 1, shall be completed, prior to commencement of use (as hereby approved) and shall be maintained at all times thereafter (including the replacement of diseased or dying plants and the removal of weeds and pest plants) to the reasonable satisfaction of Council.

Reason: To ensure the site is landscaped so as to enhance the visual and environmental amenity of the locality.

7. Noise measured at the nearest residential property boundary shall remain within the requirements of the Environment Protection Authority (EPA) guidelines for development adjacent to a residential area.

Reason: To ensure that noise does not cause nuisance to adjoining residential occupiers or owners.

8. All driveways, manoeuvring areas and hardstand areas identified on the Site Plan approved by Council under Development Plan Condition 1, shall consist of brick paving, concrete or bitumen to a standard appropriate for the intended traffic volumes and vehicle types. Individual car parking bays shall be clearly line-marked. Driveways and car parking areas shall be established prior to the commencement of use (as hereby approved) and shall be maintained at all times to the reasonable satisfaction of Council.

Reason: To ensure access and car parking is provided on the site in a manner that maintains and enhances the amenity of the locality.

9. The car parking layout and associated aisle widths and car park manoeuvring area shall be designed and constructed to comply with AS 2890.1 – Off-street parking, Part 1 and Austroads “Guide to Traffic Engineering Practice Part 11 – Parking” and AS 2890.2 – Facilities for Commercial Vehicles.

Reason: To ensure that the development complies with Standards and Best Engineering Practice.

10. The developer shall employ measures to eliminate dust emission from the site during the construction period so as not to cause nuisance to adjacent or nearby properties.

Reason: To preserve the amenity of the locality during construction work.

11. Except where otherwise approved, outside lighting shall be restricted to that necessary for security purposes only and shall be directed and shaded to prevent light overspill and/or nuisance to adjacent occupiers or distraction to drivers on adjacent public roads.

Reason: To ensure that lighting does not cause nuisance or danger to adjoining occupiers or road users thereby reducing the amenity of the locality and/or making road use unsafe.

Commissioner of Highways

12. All vehicles shall enter and exit the site in a forward direction.
13. The entry and exit points shall be suitably signed and line-marked to reinforce the desired traffic flow.

Advice Notes

- Except where otherwise varied by this Consent, the conditions imposed herein shall be in addition to conditions that apply to the subject property from previous approvals that remain active.
- The structure and design of the food premises shall be constructed in compliance with the Food Act 2001 and Food Safety Standard 3.2.3.
- The Applicant/owner is required to notify Council when a food business becomes operational. Please contact Council's Environmental Health Officers for information and requirements.
- The applicant is reminded of its general environmental duty, as required by Section 25 of the *Environment Protection Act 1993*, to take all reasonable and practicable measures to ensure that the activities on the whole site, including during construction, do not pollute the environment in a way which causes or may cause environmental harm.
- EPA information sheets, guidelines documents, codes of practice, technical bulletins etc. can be accessed on the following web site: <http://www.epa.sa.gov.au>
- The applicant is reminded that construction is required to be carried out so that it complies with the mandatory construction noise provisions contained in Part 6, Division 1 of the Environment Protection (Noise) Policy 2007 and the provisions of the *Local Nuisance and Litter Control Act 2016*. Under the *Local Nuisance and Litter Control Act 2016*, construction noise is declared a local nuisance as follows:

The noise has travelled from the location of the construction activity to neighbouring premises –

- On any Sunday or public holiday; or
- After 7pm or before 7am on any other day.

Commissioner of Highways Advice Notes

- This property abuts a section of Port Wakefield Road that was proclaimed a controlled-access road on 13 June 1968 pursuant to Part 2A of the Highways Act 1926. The Department has authorized the use of the existing access point on Port Wakefield Road to service the site.
- All signage is to be consistent with the Department of Infrastructure and Transport's publication 'Advertising Signs: Assessment Guidelines for Road Safety'.

CO-ORDINATION

Officer: MDS GMCID
Date: 12/01/2021 13/01/2021

ATTACHMENTS

This document should be read in conjunction with the following attachments:

1. Proposal Plans and Supporting Documentation including Statement of Support and Statement of Effect
2. Notice of Category 3 Development
3. Schedule 8 Referral Response
4. Extract of Relevant Development Plan Provisions and Location Maps (Consolidated 4 April 2019)

Attachment 1
Proposal Plans and
Supporting Documentation Including
Statement of Support and
Statement of Effect

ARCHITECTURAL DRAWINGS

- A001 B 3D VIEWS
- A041 A OVERALL SITE PLAN
- A062 B SITE PLAN
- A067 A CAR SWEEP PATH
- A068 A TRUCK SWEEP PATH
- A101 B PROPOSED FLOOR PLAN
- A201 B NORTH & EAST BUILDING ELEVATIONS
- A202 B WEST & SOUTH BUILDING ELEVATIONS
- A205 A FINISHES SCHEDULE
- A801 B SITE SIGNAGE PLAN
- A806 B SIGNAGE DETAILS
- A807 A SIGNAGE DETAILS



McDONALDS BOLIVAR
DA ISSUE - AUGUST 2020

General Notes

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Project Manager
 NDG
 NATIONAL DEVELOPMENT GROUP

Richmond+Ross
 CONSULTING ENGINEERS AND PROJECT MANAGERS

Consultants

Project
 McDONALDS BOLIVAR

Location
 859-885 PORT WAKEFIELD ROAD
 BOLIVAR
 SA 5110

COVER PAGE
 Project Number Drawing Number Issue
 190029 A000 B



PLAYPLACE



CORRAL



DRIVETHRU



McCAFE

Revisions	General Notes	Drawing Notes
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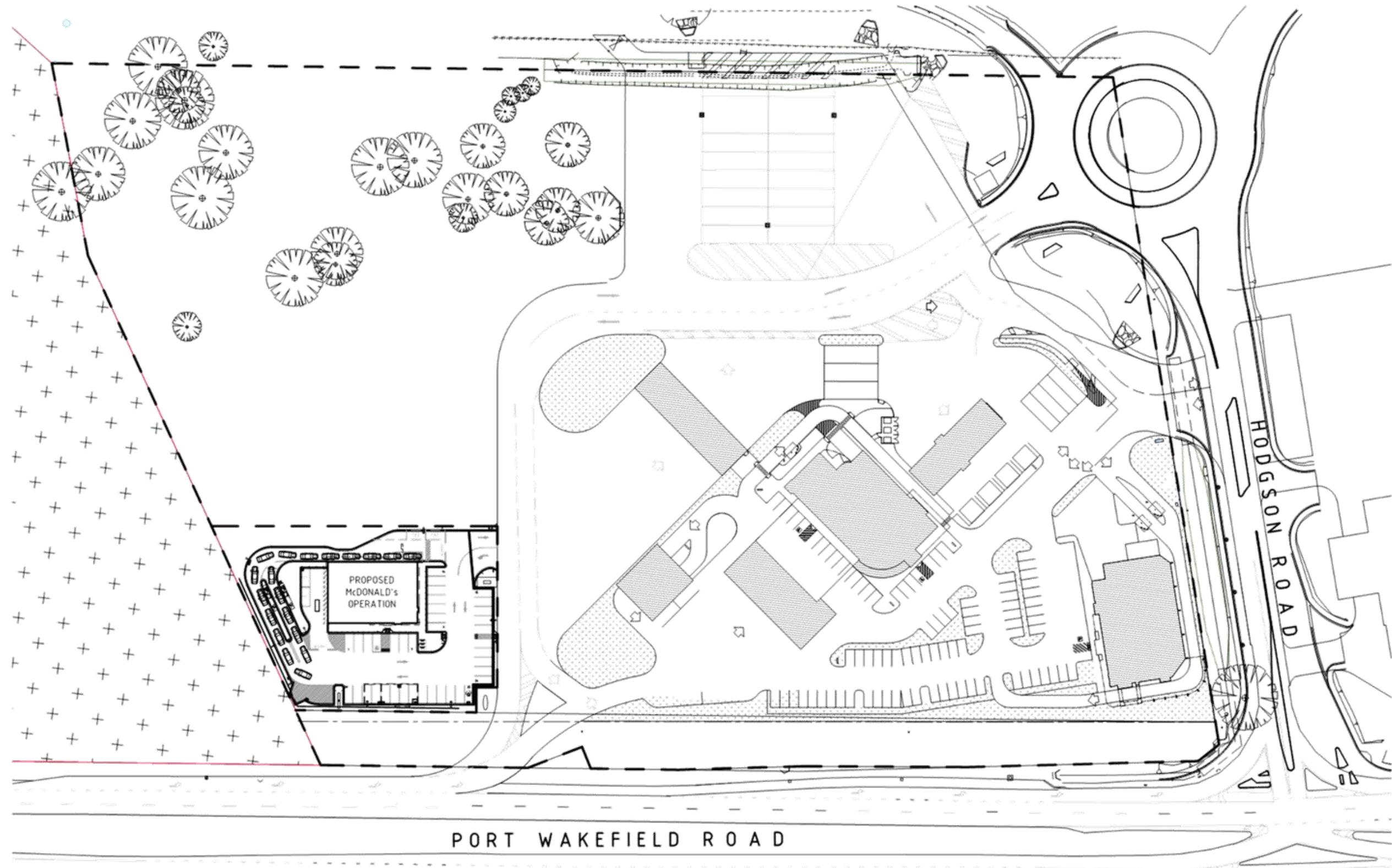
Client
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 A/N: 43 009 406 525
 100 1075 0806
 Project Manager
 NATIONAL DEVELOPMENT GROUP

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 Consultants

Project
 McDONALDS BOLIVAR
 Location
 858-855 PORT WAKEFIELD ROAD
 BOLIVAR
 SA 5110

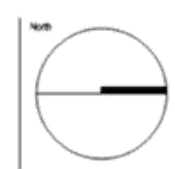
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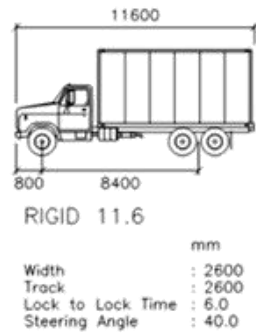
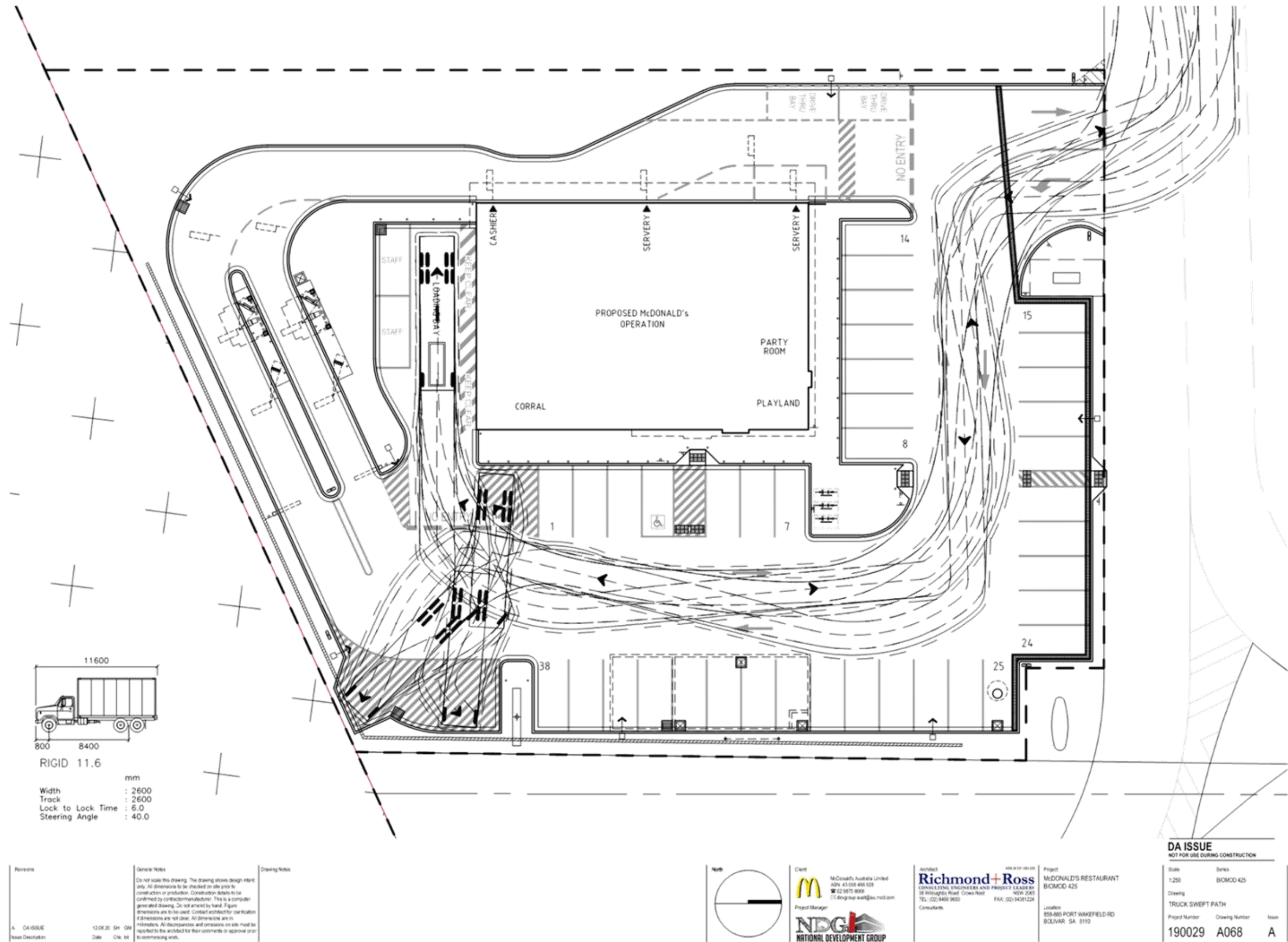
Architect
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Project
 McDONALD'S RESTAURANT
 BIOMOD 425

Location:
 109-105 PORT WAKEFIELD RD
 SOLVAY, SA 5110

DA ISSUE		
NOT FOR USE DURING CONSTRUCTION		
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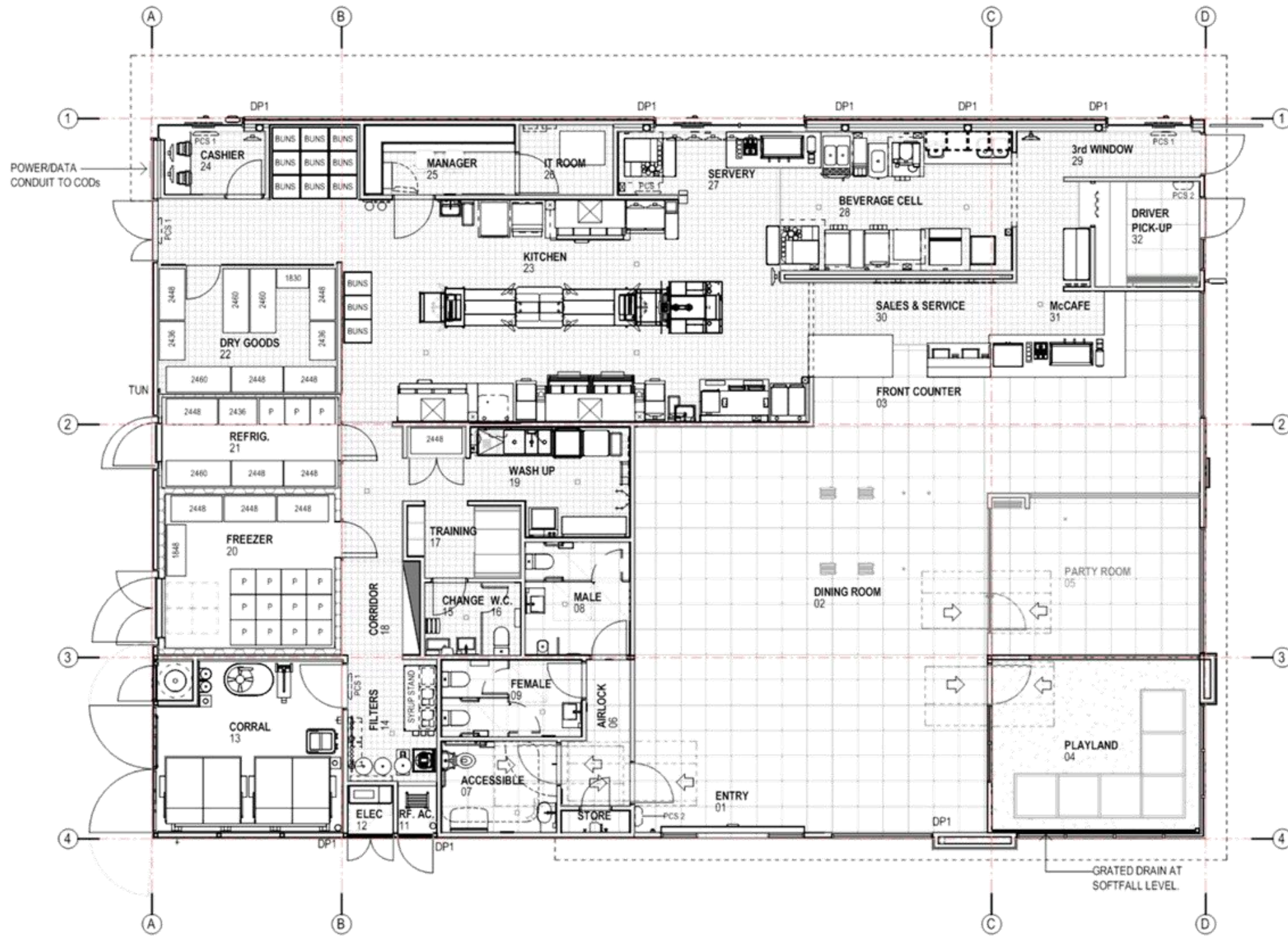
Item 8.1.1 - Attachment 1 - Proposal Plans and Supporting Documentation including Statement of Support and Statement of Effect



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 Track : 2600
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 Steering Angle : 40.0

Revisions	General Notes	Drawing Notes
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12.08.20 SH GM	Date Ck: H	

	<p>Client McDonald's Australia Limited A/N: 43 054 494 528 ☎ 02 9875 9959 T: design@au.mcd.com</p>	<p>Architect Richmond+Ross CONSULTING ENGINEERS AND PROJECT LEADERS 38 Wiltshire Road, Crown Hill NSW 2065 TEL: (02) 9450 9600 FAX: (02) 9430 1204</p>	<p>Project McDONALD'S RESTAURANT BIOMOD 425</p> <p>Location 859-865 PORT WAKEFIELD RD SOLVAY, SA 5110</p>	<p>DA ISSUE NOT FOR USE DURING CONSTRUCTION</p> <p>Scale Series 1:250 BIOMOD 425</p> <p>Drawing TRUCK SWEEP PATH</p> <p>Project Number Drawing Number Issue 190029 A068 A</p>
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Revisions	General Notes	Drawing Notes
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10/08/20 SH GH 10/08/20 SH GH Date CHG. BY		

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Project
 McDONALDS BOLIVAR
 Location
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 BOLIVAR SA 5110

DA ISSUE		
DO NOT USE DURING CONSTRUCTION		
Scale	Series	
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Drawing		
PROPOSED FLOOR PLAN		
Project Number	Drawing Number	Issue
190029	A101	B

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Revisions	General Notes	Drawing Notes
# CH. DATE A CH. DATE Issue Description Date CH. BY	Do not scale this drawing. The drawing shows design intent only. All dimensions to be checked on site prior to construction or production. Construction details to be confirmed by contractor/manufacturer. This is a computer generated drawing. Do not amend by hand. Figure dimensions are to be used. Contact architect for clarification if dimensions are not clear. All dimensions are in millimeters. All discrepancies and omissions on site must be reported to the architect for their comments or approval prior to commencing work.	1. REFER TO MASTER LEGENDS ON DA021, FINISHES LEGENDS ON DA206, SIGNAGE PLAN ON DA801



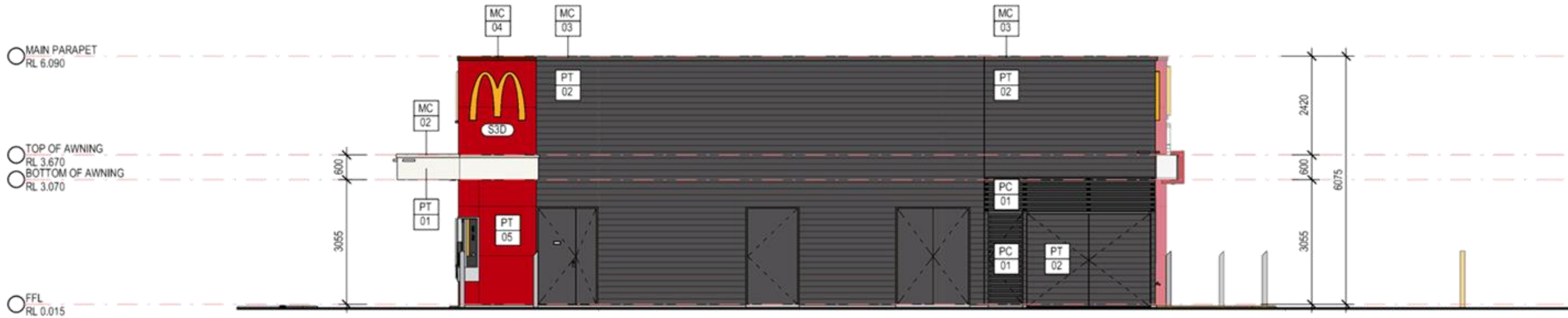
Project: MCDONALDS BOLIVAR
 Location: 855-855 PORT WAKEFIELD ROAD
 BOLIVAR SA 5110

DA ISSUE		
DO NOT USE DURING CONSTRUCTION		
Scale	Series	Issue
1:100 @ A3	B10M00	
Drawing: NORTH & EAST BUILDING ELEVATIONS		
Project Number: 190029	Drawing Number: A201	Issue: B

File Name: C:\Users\stef@ndg\OneDrive - Richmond & Ross\Documents\190029_MCDONALDS-BOLIVAR-BD000 - SITE_R2018_sam@richmondross.com.au.rvt



3 WEST ELEVATION
1:100



4 SOUTH ELEVATION
1:100

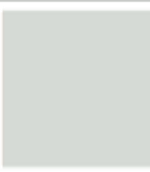

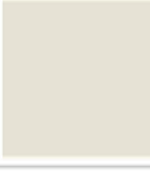



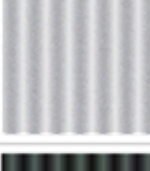

Revisions	General Notes	Drawing Notes
# CHG: [] A CHG: [] Issue Description Date CHG: []	Do not scale this drawing. The drawing shows design intent only. All dimensions to be checked on site prior to construction or production. Construction details to be confirmed by contractor/manufacturer. This is a computer generated drawing. Do not amend by hand. Figure dimensions are to be used. Contact architect for clarification if dimensions are not clear. All dimensions are in millimeters. All discrepancies and omissions on site must be reported to the architect for their comments or approval prior to commencing work.	1. REFER TO MASTER LEGENDS ON DA021, FINISHES LEGENDS ON DA205, SIGNAGE PLAN ON DA801




Project: MCDONALDS BOLIVAR
 Location: 855-855 PORT WAKEFIELD ROAD
 BOLIVAR SA 5110

DA ISSUE		
DO NOT USE DURING CONSTRUCTION		
Scale	Series	Issue
1:100 @ A3	B10M00	
Drawing: WEST & SOUTH BUILDING ELEVATIONS		
Project Number: 190029	Drawing Number: A202	Issue: B

File Name: C:\Users\stef@ndc\OneDrive - Richmond & Ross\Documents\190029_MCDONALDS-BOLIVAR-B0M00_SITE_R2018_sam@richmondross.com.au.rvt

EXTERNAL FINISHES SCHEDULE						
CODE	No.	AREA	DESCRIPTION	MANUFACTURER	COLOUR	IMAGE
AN	01	ALUMINIUM WINDOWS & DOOR FRAMES	PREFABRICATED ALUMINIUM FRAMING	CAPRAL	NATURAL FINISH CLEAR ANODISED	
MC	01	PARAPET CAPPING	PREFINISHED METAL CAPPING / FLASHING	COLORBOND	JASPER	
MC	02	PARAPET CAPPING	PREFINISHED METAL CAPPING / FLASHING	COLORBOND	SURFMIST	
MC	03	PARAPET CAPPING	PREFINISHED METAL CAPPING / FLASHING	COLORBOND	WOODLAND GREY	
MC	04	PARAPET CAPPING	PREFINISHED METAL CAPPING / FLASHING	COLORBOND	MANOR RED	
MWC	01	PLAYPLACE & PARAPETS	TIMBER LOOK ALUMINIUM CLADDING SYSTEM USING KNOTWOOD 200mm CLADDING PROFILE	KNOTWOOD	LIGHT OAK	
MWC	02	ROOF WELL (INTERNAL PARAPET LINING)	CUSTOM ORB CORRUGATED STEEL RIVET FIXED VERTICALLY TO FRAMES	LYSAGHT	ZINCALUME	
MWC	03	ROOF WELL (PLAYPLACE WALL LINING)	CUSTOM ORB CORRUGATED STEEL RIVET FIXED VERTICALLY TO FRAMES	LYSAGHT	WOODLAND GREY	

EXTERNAL FINISHES SCHEDULE						
CODE	No.	AREA	DESCRIPTION	MANUFACTURER	COLOUR	IMAGE
PC	01	CORRAL BATTENS & ROOF ACCESS, ELEC. ROOM DOORS	POWDERCOAT FINISH	DULUX DURALLOY	MONUMENT SATIN (COLORBOND)	
PT	01	FASCIAS (RIBBON)	PAINT FINISH. REFER SPECIFICATION FOR DETAILS ON PAINT TYPE & APPLICATION	DULUX	VIVID WHITE PW1H9	
PT	02	MAIN BUILDING WALLS	PAINT FINISH. REFER SPECIFICATION FOR DETAILS ON PAINT TYPE & APPLICATION	DULUX	WAYWARD GREY PG1G8	
PT	05	BLADE WALL & DRIVETHRU WINDOWS	PAINT FINISH. REFER SPECIFICATION FOR DETAILS ON PAINT TYPE & APPLICATION	DULUX	McDONALDS RED RGB Value: R189 G0 B22.	
STN	01	DRIVETHRU WINDOW SILL & SURROUND	RECONSTITUTED STONE. REFER TO DECOR DOCUMENTS	REFER DECOR	REFER DECOR	

Revisions

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Drawing Notes
 1. FOR BUILDING ELEVATIONS REFER A201-A203

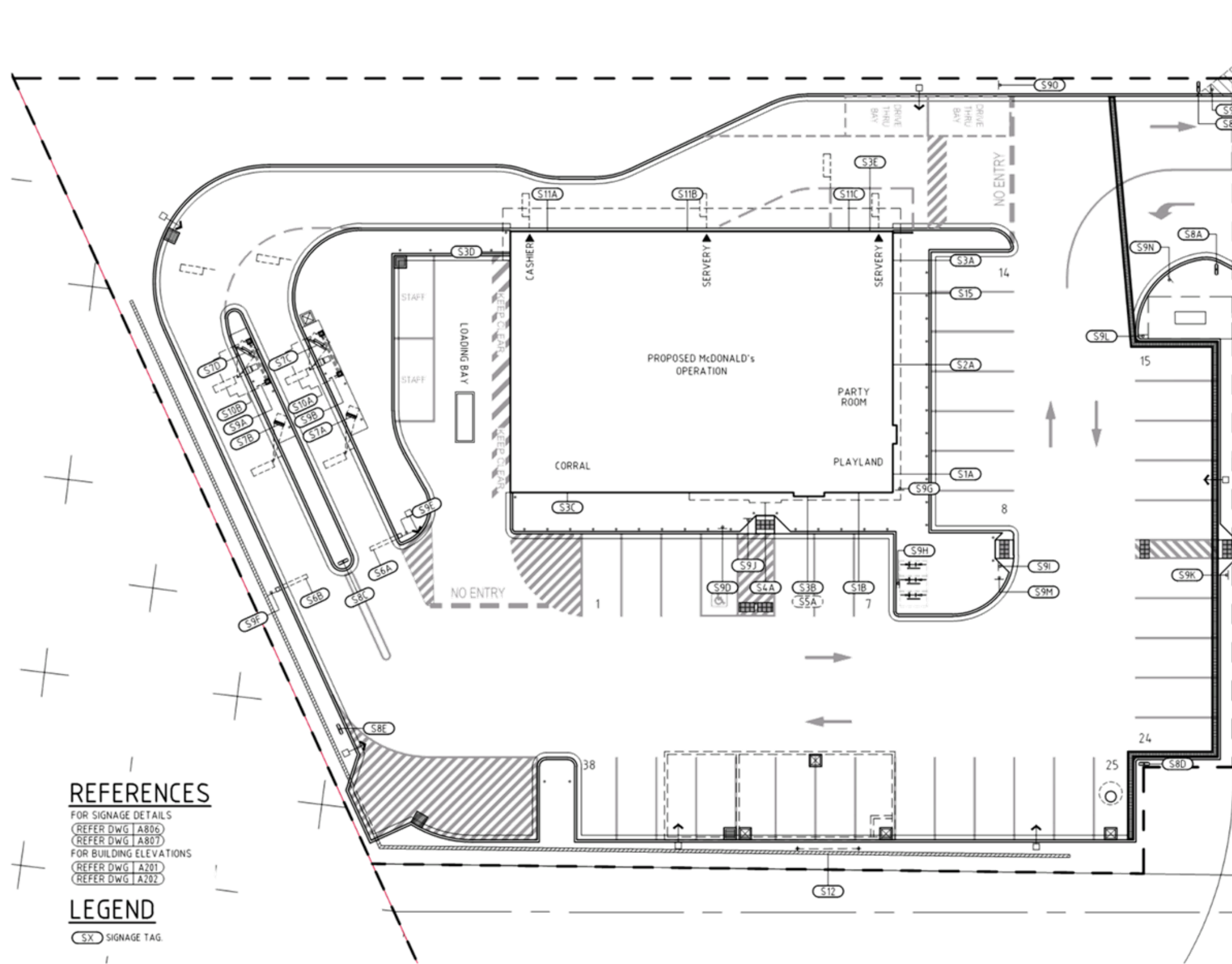


Richmond+Ross
 CONSULTING ENGINEERS AND PROJECT LEADERS
 38 Willoughby Road, Coles Hill, NSW 2065
 Tel: (02) 9490 9600 Fax: (02) 9490 1204
 Consultants

Project
 McDONALDS BOLIVAR
 Location:
 859-865 PORT WAKEFIELD ROAD
 BOLIVAR
 SA 5110

DA ISSUE
 DO NOT USE DURING CONSTRUCTION
 Scale: 1:100 @ A3
 Series: B10A00
 Drawing: FINISHES SCHEDULE
 Project Number: 190029
 Drawing Number: A205
 Issue: A

File Name: C:\Users\stph\OneDrive - Richmond & Ross\Documents\190029_MCDONALDS-BOLIVAR-BDACC_SITE_R2016_sdm\pdrawings\com.au\rvf



SIGNAGE SCHEDULE		ILLUMINATION
TAG	DESCRIPTION	
S1A, S1B	PLAY PLACE SIGN LOGO & LETTERING FIXED TO HANGING FRAME.	YES
S2A	WALL SIGN, Ø1200 BUTTON SIGN WORDING: "McCAFE" FIXED TO WALL.	
S3A, S3B, S3C, S3D, S3E	WALL SIGN, 1.371 X 1.200m HIGH "M" LOGO FIXED TO WALL/BLADE.	
S4A	ENTRY CLIP, 2.100 X 0.750m HIGH NON-ILLUMINATED FASCIA WITH LED ILLUMINATED LETTERING "McDONALD'S"	
S5A	WALL SIGN, 1.420 X 0.70m HIGH WORDING: "McCAFE" FIXED TO BLADE.	
S6A, S6B	HEIGHT CLEARANCE GANTRY.	
S7A, S7B	SINGLE DIGITAL PRESELL MENUBOARD	
S7C, S7D	DOUBLE DIGITAL MENUBOARD	
S8A	DIRECTIONAL SIGN, 0.7 X 2.3m HIGH SINGLE SIDED. WORDING: "WELCOME".	
S8B	DIRECTIONAL SIGN, 0.7 X 2.3m HIGH SINGLE SIDED. WORDING: "THANK YOU".	
S8C	DIRECTIONAL SIGN, 0.7 X 2.3m HIGH SINGLE SIDED. WORDING: "DRIVE THRU" & "ANY LANE, ANY TIME".	
S8D, S8E	DIRECTIONAL SIGN, 0.7 X 2.3m HIGH SINGLE SIDED. WORDING: "DRIVE THRU".	
S9A, S9B, S9C	STOP SIGN AS SPECIFIED.	NO
S9D	ACCESSIBLE PARKING AS SPECIFIED.	
S9E, S9F, S9G	NO PEDESTRIAN ACCESS SIGN AS SPECIFIED.	
S9H	BIKE RACK AS SPECIFIED (DOUBLE SIDED WHERE SHOWN).	
S9I, S9J, S9K	PEDESTRIAN WARNING SIGN AS SPECIFIED.	
S9L, S9M	PEDESTRIAN CROSSING SIGN AS SPECIFIED (DOUBLE SIDED WHERE SHOWN).	
S9N	SPEED SIGN (10km/hr) AS SPECIFIED. SHARED ZONE.	
S9O	GIVE WAY SIGN AS SPECIFIED.	
S10A, S10B	WORDING: "1. ORDER HERE". REFER DWG. A082.	
S11A, S11B, S11C	INFORMATION SIGNS WORDING: "2. PAY HERE" / "3. PICK UP HERE" / "4. PICK UP HERE".	
S12	FLAGPOLES AND BANNER.	
S14	WARNING SIGN FOR CONFINED SPACES AS SPECIFIED. 0.45 X 0.30m. LOCATION AND NUMBER OF SIGNS SHALL BE DETERMINED BY THE McDONALD'S DEVELOPMENT MANAGER TO SUIT SITE SPECIFIC REQUIREMENTS.	NO
S15	WALL SIGN, 0.40 X 0.40m HIGH. "McDELIVERY" SIGN FIXED TO WALL.	YES

REFERENCES
 FOR SIGNAGE DETAILS
 REFER DWG | A806
 REFER DWG | A807
 FOR BUILDING ELEVATIONS
 REFER DWG | A201
 REFER DWG | A202

LEGEND
 (SX) SIGNAGE TAG.

Revisions	General Notes	Drawing Notes
B DA ISSUE 17.08.20 SH GM	Do not scale this drawing. The drawing shows design intent only. All dimensions to be checked on site prior to construction or production. Construction details to be confirmed by contractor/manufacturer. This is a computer generated drawing. Do not amend by hand. Figure dimensions are to be used. Contact architect for clarification if dimensions are not clear. All dimensions are in millimeters. All discrepancies and omissions on site must be reported to the architect for their comments or approval prior to commencing work.	
A DA ISSUE 12.08.20 SH GM		
Issue Description	Date	Chk. Int.

North

Client: McDonald's Australia Limited
 AIN 43 088 486 528
 02 9870 8068
 17 denigup@aia.com.au

Project Manager: NDC NATIONAL DEVELOPMENT GROUP

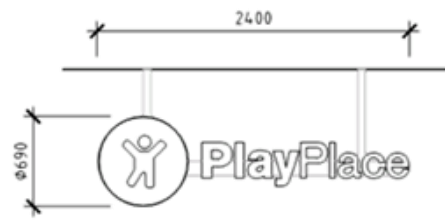
Architect: Richmond+Ross
 CONSULTING ENGINEERS AND PROJECT MANAGERS
 18 Wroughtley Road, Crown Hill NSW 2065
 TEL: (02) 9450 9000 FAX: (02) 9436 1204

Project: McDONALD'S RESTAURANT BIOMOD 425
 Location: 105-106 PORT WAKEFIELD RD BOLLAR, SA 5110

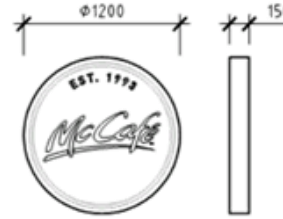
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 Drawing: SITE SIGNAGE PLAN
 Project Number: 190029
 Drawing Number: A801
 Issue: B

DA ISSUE
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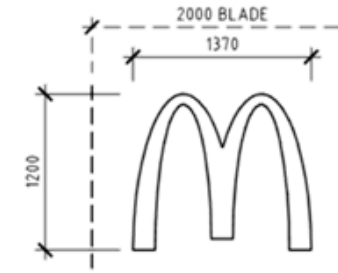
Item 8.1.1 - Attachment 1 - Proposal Plans and Supporting Documentation including Statement of Support and Statement of Effect



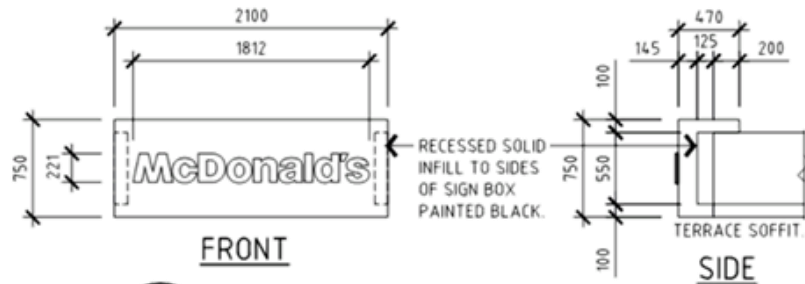
S1 HANGING SIGN
A801 YELLOW LOGO WITH INDIVIDUAL POLYCARBONATE LETTERING FIXED TO FRAME. INTERNALLY ILLUMINATED. WHITE LETTERS.



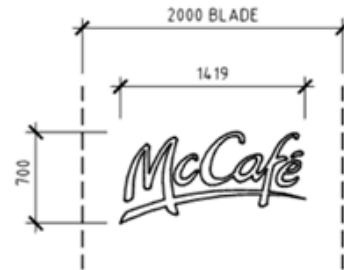
S2 WALL SIGN
A801 FABRICATED METAL BUTTON SIGN. OPAL FACES WITH BLACK VINYL GRAPHICS. LED ILLUMINATION.



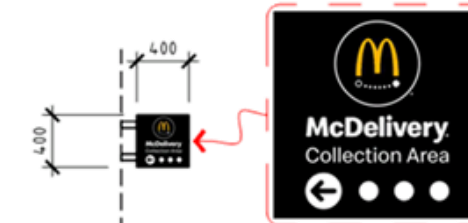
S3 WALL/BLADE SIGN
A801 YELLOW FLAT FACE POLYCARBONATE LOGO. INTERNALLY ILLUMINATED. YELLOW LOGO ON SOLID RED BACKGROUND.



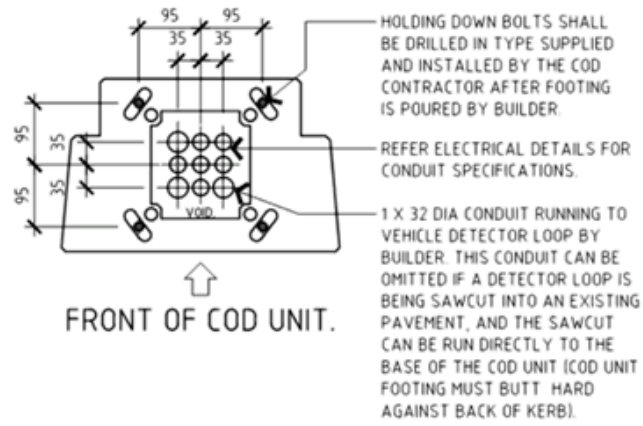
S4 ENTRY CLIP
A801 LED ILLUMINATED WORDMARK ONLY. RED CLIP TO BE CONSTRUCTED BY THE BUILDER.



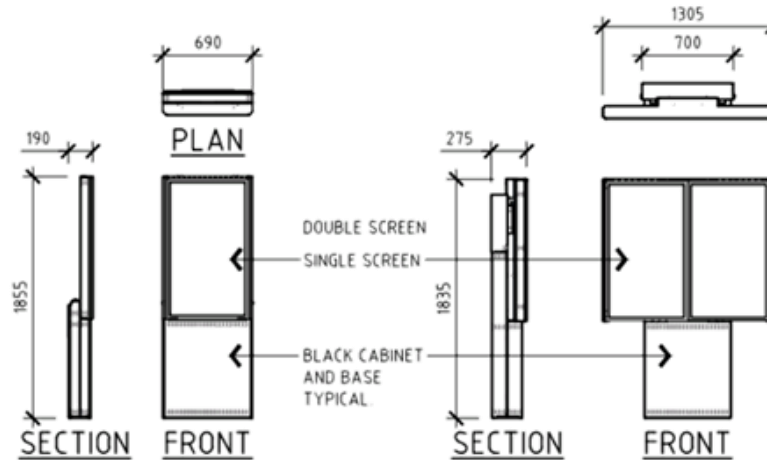
S5 BLADE SIGN
A801 INDIVIDUAL HALO ILLUMINATED REVERSE PAN FABRICATED ALUMINIUM CHANNEL LETTERS. BRUSHED ALUMINIUM FINISH.



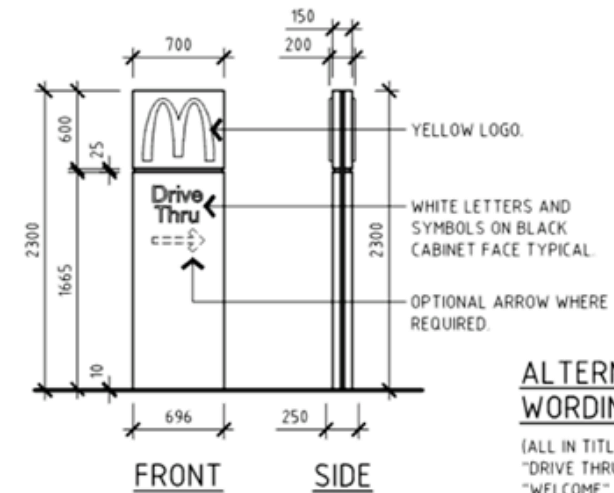
S15 WALL SIGN
A801 FABRICATED METAL BUTTON SIGN. OPAL FACES WITH BLACK VINYL GRAPHICS. YELLOW 'M'. LED ILLUMINATION.



COD UNIT BASE PLATE DETAIL
SCALE 1:10



S7 DIGITAL MENUBOARDS
A801



S8 DIRECTIONAL SIGN
A801

ALTERNATIVE WORDING
(ALL IN TITLE CASE):
"DRIVE THRU",
"WELCOME",
"NO ENTRY",
"THANK YOU",
"ANY LANE, ANY TIME"

Revisions	General Notes	Drawing Notes
B DA ISSUE A DA ISSUE Issue Description	Do not scale this drawing. The drawing shows design intent only. All dimensions to be checked on site prior to construction or production. Construction details to be confirmed by contractor/manufacturer. This is a computer generated drawing. Do not amend by hand. Figure dimensions are to be used. Contact architect for clarification if dimensions are not clear. All dimensions are in millimeters. All discrepancies and omissions on site must be reported to the architect for their comments or approval prior to commencing work.	
17.08.20 SH GA	Date	12.08.20 SH GA

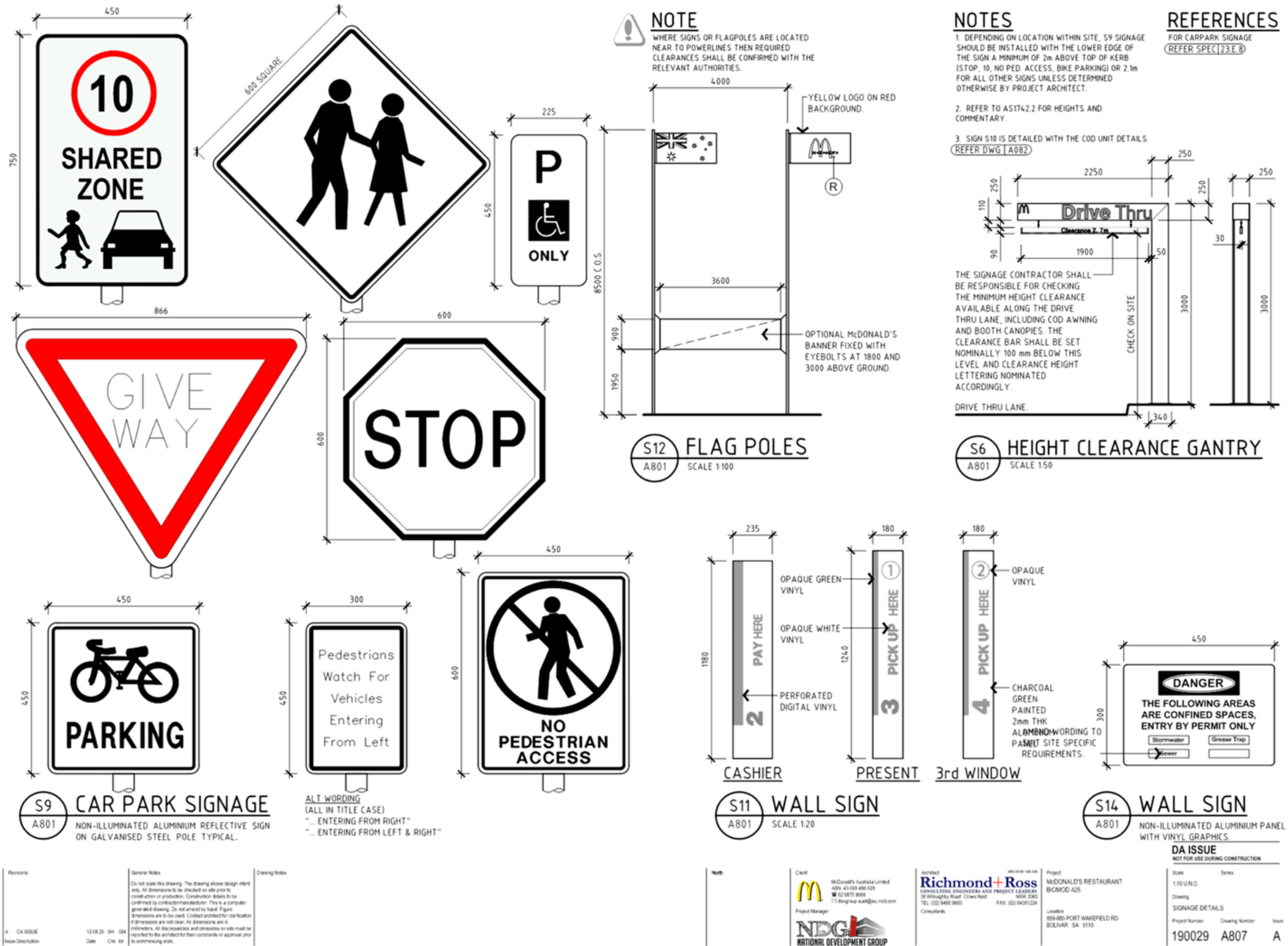
North

Client
 McDonald's Australia Limited
 ADV: 43 088 486 528
 02 9870 9908
 11 Deakin Street, Sydney NSW 2005
 Project Manager
 NATIONAL DEVELOPMENT GROUP

Architect
 Richmond+Ross
 CONSULTING ENGINEERS AND PROJECT MANAGERS
 38 Willoughby Road, Crows Nest NSW 2058
 TEL: (02) 9450 9000 FAX: (02) 94381224
 Consultants

Project
 McDONALD'S RESTAURANT
 BIOMOD 425
 Location
 858-866 PORT WAKEFIELD RD
 BOLLARUA SA 5110

DA ISSUE		
NOT FOR USE DURING CONSTRUCTION		
Scale	Series	
1:50 UNO.		
Drawing		
SIGNAGE DETAILS		
Project Number	Drawing Number	Issue
190029	A806	B



Item 8.1.1 - Attachment 1 - Proposal Plans and Supporting Documentation including Statement of Support and Statement of Effect

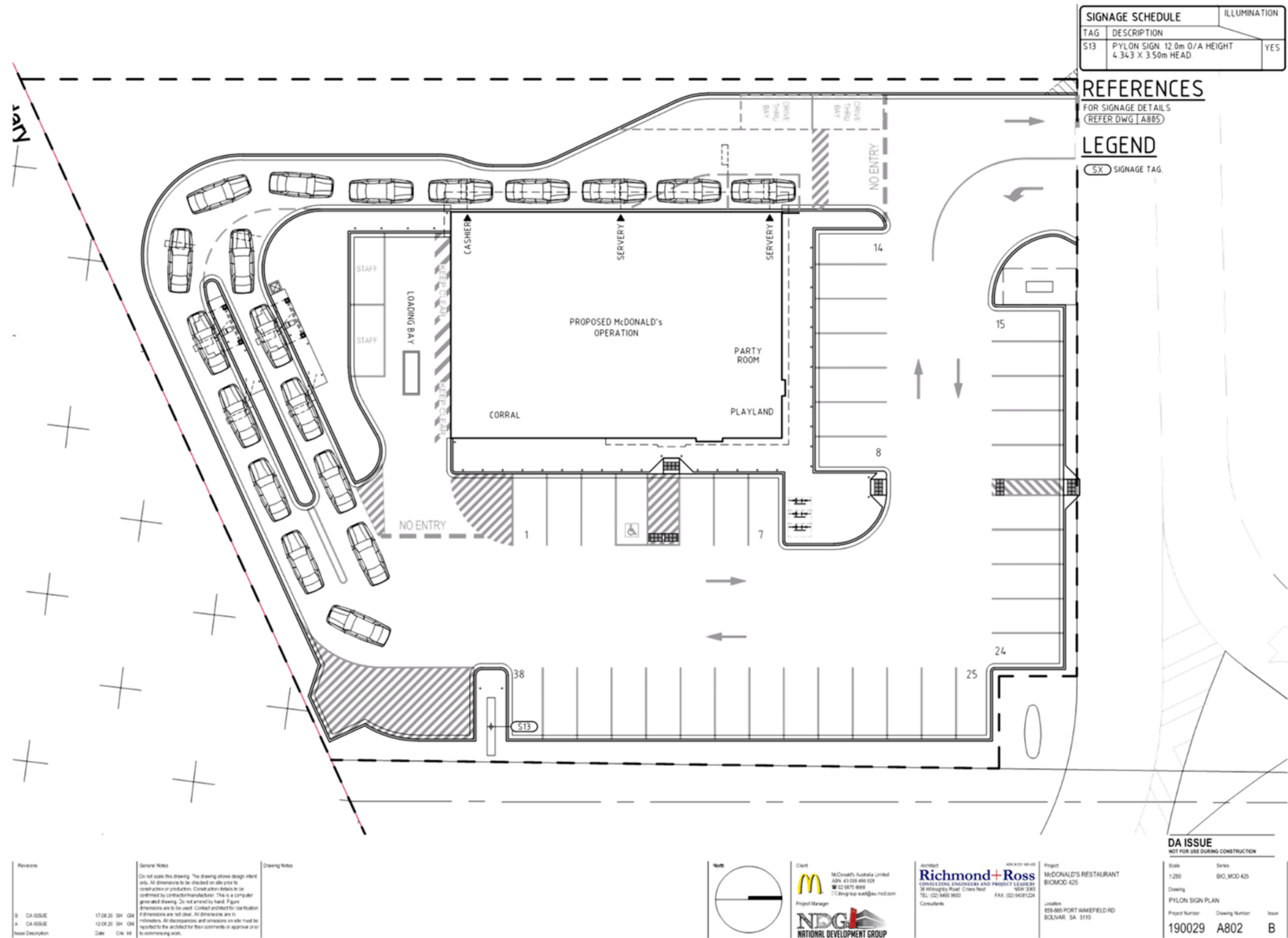
PROPOSED McDONALD'S OPERATION AT:

859-885 PORT WAKEFIELD RD
BOLIVAR
SA 5110

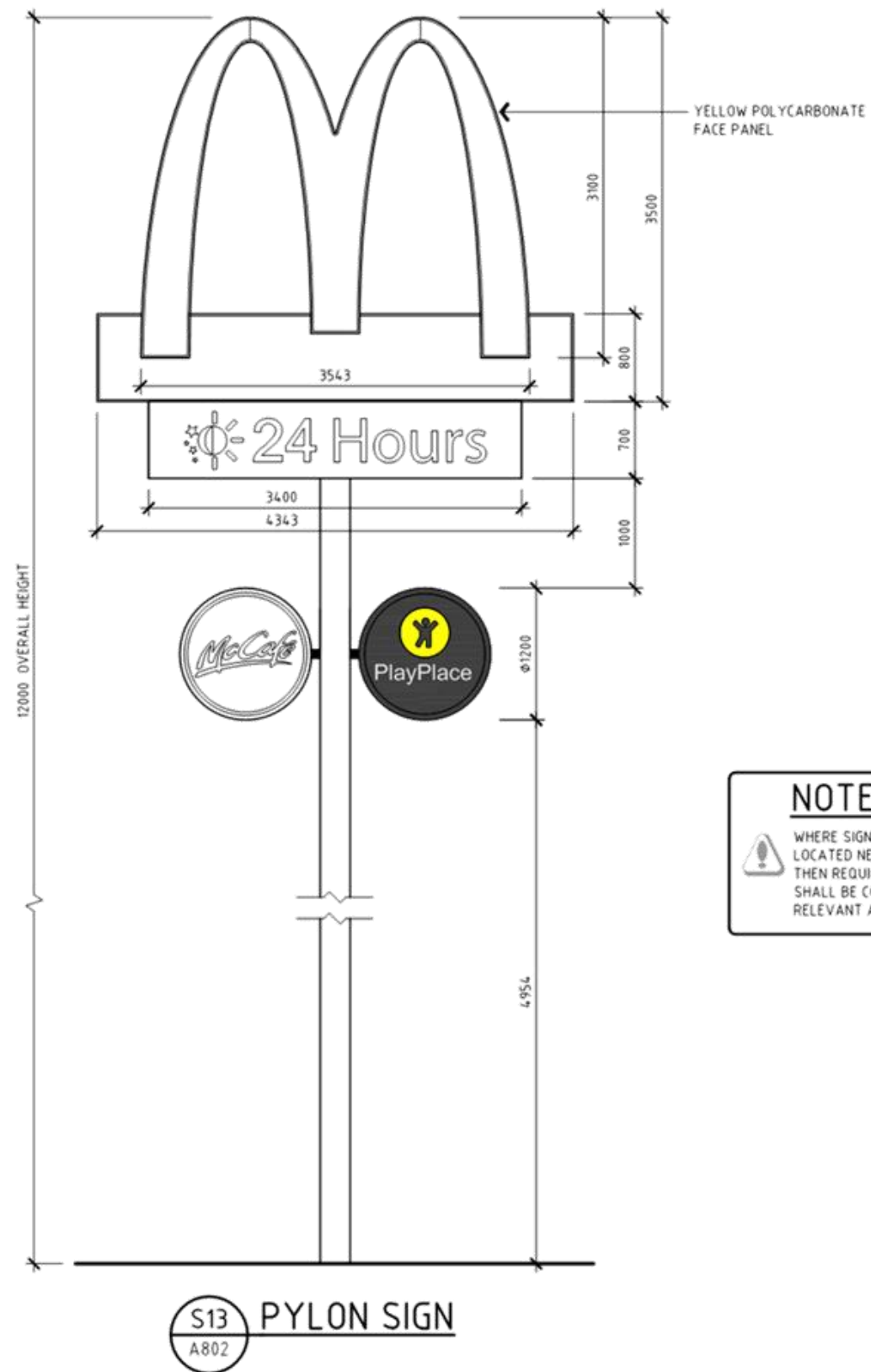
STAGE:

DA ISSUE (PYLON SIGN)
AMENDMENT: A





Item 8.1.1 - Attachment 1 - Proposal Plans and Supporting Documentation including Statement of Support and Statement of Effect



NOTE

WHERE SIGNS OR FLAGPOLES ARE LOCATED NEAR TO POWERLINES THEN REQUIRED CLEARANCES SHALL BE CONFIRMED WITH THE RELEVANT AUTHORITIES.

Revisions	General Notes	Drawing Notes
A DA ISSUE	Do not scale this drawing. The drawing shows design intent only. All dimensions to be checked on site prior to construction or production. Construction details to be confirmed by contractor/manufacturer. This is a computer generated drawing. Do not amend by hand. Figure dimensions are to be used. Contact architect for clarification if dimensions are not clear. All dimensions are in millimeters. All discrepancies and omissions on site must be reported to the architect for their comments or approval prior to commencing work.	
XX 25 20 SH GM		
Date	Chk	Iss

North

Client

McDonald's Australia Limited
 ABN 43 008 496 528
 02 9875 9959
 11 Denmore Road, Sydney NSW 1500

Project Manager

NATIONAL DEVELOPMENT GROUP

Architect

RICHMOND+ROSS
 CONSULTING ENGINEERS AND PROJECT MANAGERS
 38 Kingsley Place, Crown Hill NSW 2061
 TEL: (02) 9450 9020 FAX: (02) 94381224

Consultants

Project

MCDONALD'S RESTAURANT
 BIOMOD 425

Location

859-865 PORT WAKEFIELD RD
 SOLVAY, SA 5110

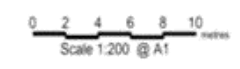
DA ISSUE		
NOT FOR USE DURING CONSTRUCTION		
Scale	Series	
1:50 UNO.		
Drawing		
PYLON SIGN DETAILS		
Project Number	Drawing Number	Issue
190029	A805	A



- LEGEND**
- Existing Tree to be Removed
 - Proposed Trees (refer to planting schedule)
 - Proposed Shrubs (refer to planting schedule)
 - Proposed Low Grasses (refer to planting schedule)
 - Mulched Gardenbed (Mossrock 'Hogged Bark' or similar)
 - Footpath (Concrete surface)
 - Gravel surface

PLANT IMAGES

CODE	BOTANIC NAME	COMMON NAME	SIZE (MATURITY)	RECOMMEND POT SIZE	QUANTITY
TREES					
Am	Acacia melanoxylon	Blackwood	12-15 x 5	45L	4
Pca	Pyrus calleryana 'Aristocrat'	Callery pear	12 x 8	45L	5
SHRUBS					
Ca	Callistemon viminalis 'Little John'	Bottlebrush	1.0 x 1.0	140mm	145
Cv	Callistemon viminalis 'Scarlet Flame'	Bottlebrush	1.0 x 0.8	140mm	7
Wt	Westringia 'Wynyardie Gem'	Coastal Rosemary	1.5 x 2.0	140mm	41
GRASSES & GROUNDCOVERS					
Asl	Atriplex suberecta	Lagoon saltbush	0.2 x 0.8	140mm	5
Bm	Brachycome multifida	Cut leaf daisy	0.4 x 0.6	140mm	36
Cc	Calceophalus citreus	Lemon beauty heads	0.4 x 0.8	140mm	44
Cr	Carpobrotus rosali	Pigface	0.2 x 1.0	140mm	42
Dr	Dianella revoluta	Pale-anther Flax Lily	0.5 x 0.5	140mm	125
Ll	Lomandra longifolia	Spiny-head Mat Rush	0.7 x 0.7	140mm	15
Po	Patersonia occidentalis	Purple Flag	0.7 x 0.5	140mm	52
Pi	Poa labillardieri	Tussock Grass	1.0 x 0.8	140mm	28
Sc	Santolina chamaecyparissus	Lavender Cotton	0.5 x 1.0	140mm	90



22799/LA | DETAILED LANDSCAPE PLAN REV A
 Date: 17/11/2020
 By: SHM, KAS

McDONALD'S AUSTRALIA LIMITED
 Corner Port Wakefield and Hodgson Road
 Ascot, WA

DETAILED LANDSCAPE PLAN
 L01: OVERALL LAYOUT

Item 8.1.1 - Attachment 1 - Proposal Plans and Supporting Documentation including Statement of Support and Statement of Effect

LANDSCAPE NOTES

IRRIGATION
 If irrigation is to be installed we recommend an automatic controller such as Rainbird ESP or similar. Lateral lines should be PVC and HDPE combination drip irrigation lines are to be Netafim 'Techline' or similar installed beneath mulch.
 A qualified irrigation contractor should be engaged to design the system (including controller valve and piping requirements) and to complete all irrigation works (trenching and conduit installation may be completed by the contractor).
 Conduits to planters and garden beds are to be installed beneath paving drainage outlet points are to be coordinated with the building storm water design.
 Water and electrical connection points are to be coordinated with the building design and specified on the engineers drawings.
 Upon completion the system is to be tested and commissioned prior to hand-over as-built drawings and manuals to be prepared and submitted.

DRAINAGE
 Drainage for the development will be integrated and connected into the building stormwater design.
 Proposed topping areas to be graded drained to prevent water discharge into neighbouring properties.

PLANTING
 Driveway trees to have lower limbs uplifted to maintain clear site lines and be positioned to ensure trunk is offset min. 100mm from kerb. Trees adjacent to driveway should incorporate structural soil mix within 2.0m radius under pavement.
 Trees and shrubs shall be healthy nursery stock free from insects diseases and weeds the specified plant heights and pot sizes are minimum. If plant material is unavailable in these sizes larger stock must be used. Plant substitution is not acceptable.

GENERAL NOTES
 This drawing is for Town Planning purposes only.
 Do not scale from plan - contractor to verify all dimensions on site prior to commencing construction.
 The plants used have low water requirements.
 Native plant species selection chosen for environmental appropriateness and low maintenance irrigation requirements.
 Low growing shrub forms maintain sight lines and provide colour interest.
 Neat architectural foliage to provide year-round green texture without ongoing maintenance (clipping pruning) requirements.
 Stone pebble mulch nominated in garden beds to reduce maintenance and provide neat high-end finish.

MAINTENANCE NOTES

The Landscape maintenance period is to be a minimum of 10 months duration from Practical Completion and continue until Hand Over to Council.

Maintenance shall be comprised of the following works to ensure continuous healthy growth of all vegetation and ensure the site is maintained in a tidy fashion for the duration of the Maintenance Period:

- Initial establishment (first 2 months)
 - minimum establishment
- Consolidation period (3 - 6 months)
 - minimum establishment
 - minimum maintenance
- Ongoing period (6 - 12 months)
 - minimum maintenance

Maintenance Period Commencement Immediately following Practical Completion
 Duration 10 months (10 weeks)

Actions to be undertaken All vegetation planted as part of the program of works will be regularly maintained to ensure ongoing health and establishment of the works including:

- watering
- weeding
- rubbish removal
- fertilising
- pest and disease control
- re-staking and tying
- replanting
- mulching and
- pruning.

This work will be undertaken by the landscape contractor appointed by the developer. The work is to be undertaken on a minimum basis outlined above and as required to ensure successful establishment as per the contract specifications.
 The developer will be responsible for the costs associated with the Maintenance Period until handed over to Council.

Maintenance Specification
 Maintain the landscape works intensively for a period of 1 week to ensure healthy establishment (weekly visits) followed by regular ongoing maintenance for a minimum duration of 2 months following the date of the issue of the Certificate of Practical Completion by the Superintendent. Any defects shall be rectified immediately.
 Maintenance shall include the care of the works by accepted horticultural practices as well as rectifying any defects that become apparent in the works under normal use. This shall include but shall not be limited to the following items where and as required:

- WATERING FERTILISING CULTIVATION TOP DRESSING
- RENOVATING WEEDING PESTS AND DISEASE CONTROL
- STAKING MAINTENANCE REPLACEMENT OF PLANT MATERIALS
- REPLANTING PRUNING RE-MULCHING MOWING OF GRASS
- KEEPING THE SITE NEAT AND TIDY.

Any soil subsidence or erosion which may occur after the soil filling and preparation operations shall be made good.
 All newly planted areas shall be protected from casual pedestrian traffic as specified herein. Protective fences where required shall be removed following successful establishment of the works.

All mulched surfaces shall be kept in a clean and tidy condition and be reinstated or topped up where necessary.

Defects Liability The whole of the works shall be upheld against any defects due to faulty and/or inferior quality materials and/or workmanship as per the requirements of the Head Contract.

Practical completion of planting Practical completion of the planting works includes but is not limited to the replacement of plants which have failed been damaged or been stolen during the works.

Program Furnish a proposed planting maintenance program and amend it as required. Comply with the amended program.

Log book Keep a log book recording when and what maintenance work has been done and what materials including toxic materials have been used. Make the log book available for inspection on request.

Establishment and grass Where existing planting or grass is within the landscape contract area maintain it as for the corresponding classifications of new grassland or planting.

Recurrent works Throughout the maintenance period continue to carry out recurrent works of a maintenance nature including but not limited to watering mowing weeding rubbish removal fertilising pest and disease control reseeding returfing staking and tying replanting cultivating pruning hedge clipping aerating renovating top dressing weekly mowing of grass and keeping the site neat and tidy.

Replacements Continue to replace failed damaged or stolen plants. If failed due to incorrect insufficient establishment or maintenance or technique or neglect no additional cost for replacement may be claimed.
 It is the Contractor's responsibility to demonstrate plants have been stolen vandalised.

Mulch surfaces Maintain the surface in a clean and tidy condition and reinstatement the mulch as necessary.

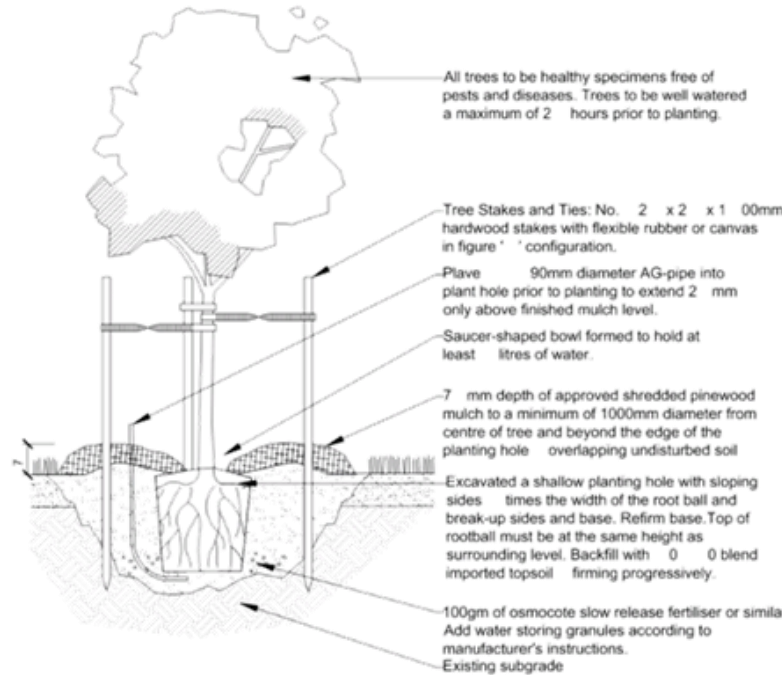
Grassed areas Carry out grass mowing throughout the contract period only as required to maintain the site in a neat healthy condition

Insect control Spray against insect and fungus infestation as required and if considered necessary by the Superintendent.
 All spraying shall be carried out in accordance with the manufacturer's directions.
 Report any occurrence of insect attack or evidence of disease amongst the plant material. The Superintendent shall be notified prior to spraying work being carried out.

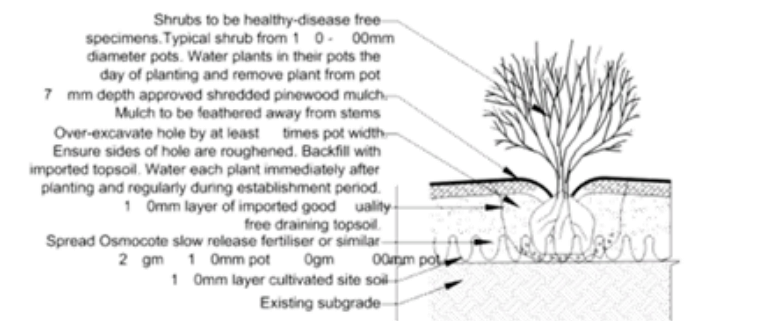
Watering All planting and garden beds are to be watered regularly to ensure continuous healthy growth. The minimum requirements shall be consistent with the natural rainfall of the site location. New planting shall receive regular and frequent deep soakings to ensure establishment and healthy growth. Watering method and technique shall accord with current water restrictions. Monitor water requirements and water adequately to ensure active growth especially during warmer months.

Garden Areas Garden beds shall be maintained in a weed free state. Any use of spot spraying or other form of weeding shall be undertaken so as not to damage plants planted as part of the contract. Any planting planted as part of the contract which is damaged by the contractor shall be replaced at the contractor's expense.
 The Contractor shall mow the grass areas at a suitable height as instructed so as to maintain healthy growth and a neat appearance. The mowing frequency may be subject to change as approved by the Superintendent due to weather and other circumstances. Other maintenance activities for grassing such as weeding reseeding and rolling etc. shall be priced separately and approved by the Superintendent. If approved grass areas to be weeded shall be sprayed with approved selective herbicide against broadleaf weeds in accordance with the manufacturer's directions.

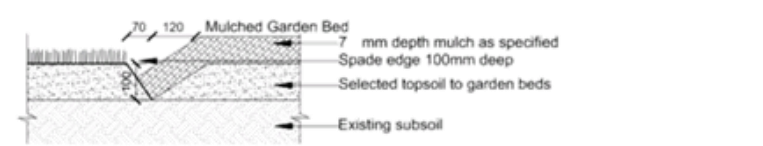
Practical completion On expiry of the Maintenance Period a site inspection shall be arranged between the Superintendent or Landscape Architect and the Landscape Contractor and Council. On approval of the works and rectification of any defects the Maintenance Period shall be deemed completed. A final Completion Certificate will then be issued and the site handed over to Council



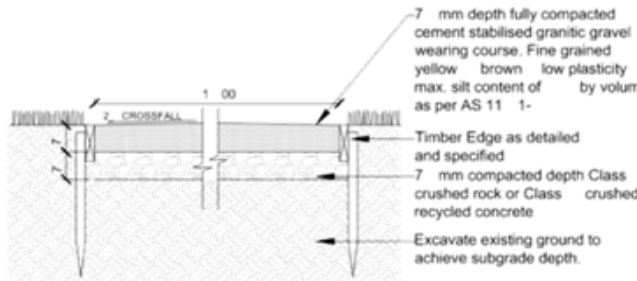
A TYPICAL TREE PLANTING DETAIL ADVANCED STOCK *not to scale*



B TYPICAL SHRUB PLANTING DETAIL *not to scale*



C TYPICAL SPADE EDGE DETAIL *not to scale*



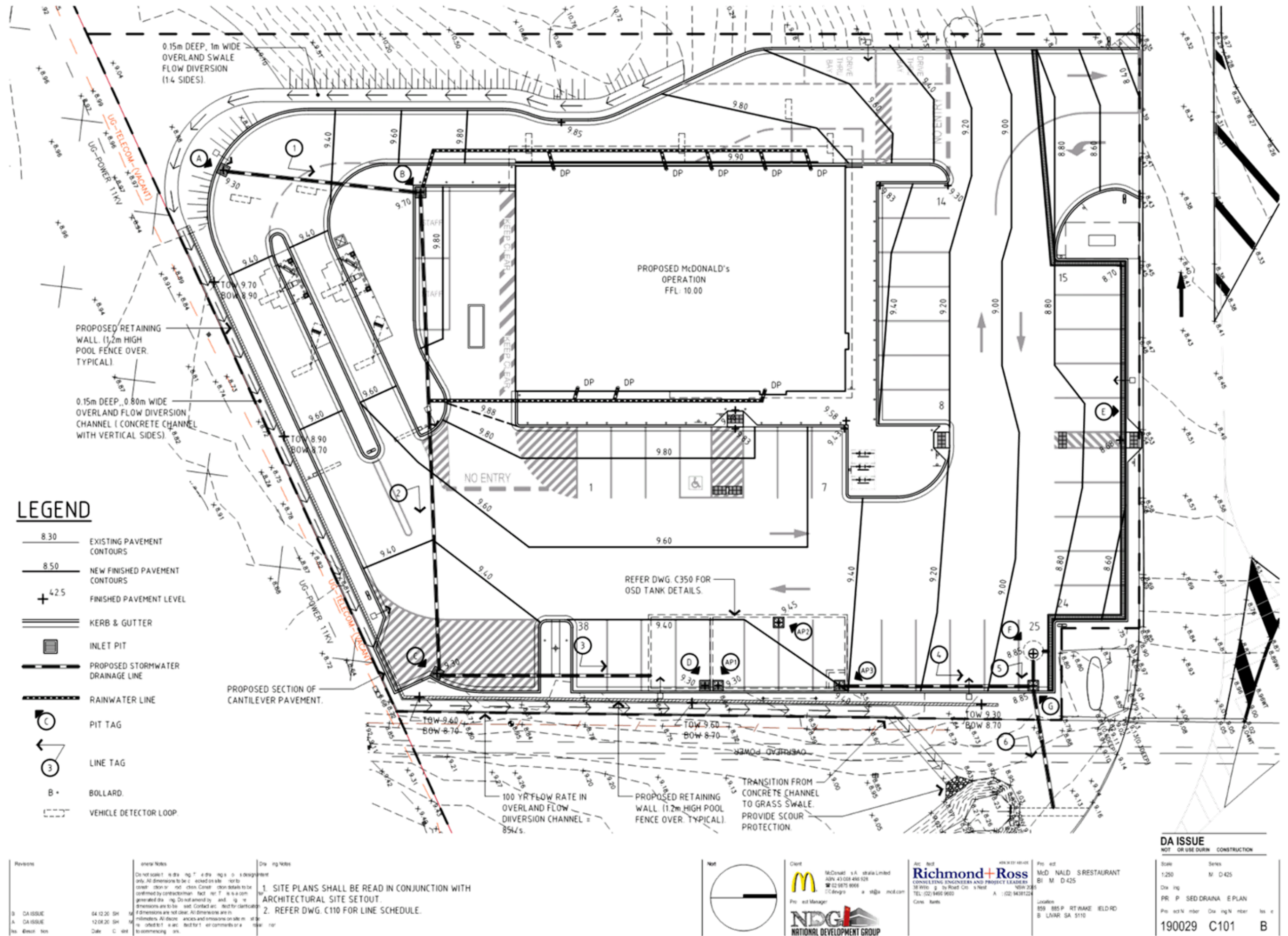
D TYPICAL GRANITIC GRAVEL DETAIL *not to scale*



22799/LA | DETAILED LANDSCAPE PLAN
 Date: 17/11/2020
 By: SHM, CHS, KAS

McDONALD'S AUSTRALIA LIMITED
 Corner Port Wakefield and Hodgson Road
 Ascot, WA

DETAILED LANDSCAPE PLANS
 L0 : LANDSCAPE DETAILS



Item 8.1.1 - Attachment 1 - Proposal Plans and Supporting Documentation including Statement of Support and Statement of Effect

STORMWATER RUNOFF CALCULATIONS

USING FORMULA $Q = 0.00028 CAI$
 WHERE Q = DISCHARGE IN LITRES PER SECOND
 C = A RUNOFF COEFFICIENT (SEE TABLE)
 A = CATCHMENT AREA IN SQ.M.
 I = RAINFALL INTENSITY IN MILLIMETRES PER HOUR
 I_5 = 181 MM/HR FOR 100 YEAR RETURN PERIOD 5 MINUTE DURATION STORM

LINE	CATCHMENT AREA (SQM) (OVERLAND FLOWS)			FLOW INTO LINE FROM CATCHMENT (L/S)			TOTAL FLOW (L/S)	MINIMUM PIPE REQUIREMENT		
	ROOF	PAVE'T	L'SCAPE	ROOF C=1.0	PAVE'T C=0.9	L'SCAPE C=0.65		SIZE (MM)	GRADE (MIN)	CAPACITY (L/S)
1	-	360	-	-	16.3	-	16.3	225	1:200	45.8
2	4.60	130	-	23.1	5.9	-	45.3	300	1:250	87.6
3	-	350	-	-	15.9	-	61.2	300	1:200	98.2
4	-	515	-	-	23.3	-	84.5	300	1:200	98.2
5	-	1060	-	-	48.0	-	48.0	225	1:100	66.6
6	-	-	-	-	-	-	132.5	450x250	1:180	14.2

LINE SCHEDULE

TAG	UPSTREAM INVERT	SIZE	MATERIAL	LENGTH	GRADE (ACTUAL)	DOWNSTREAM INVERT	NOTES
1	8.77	225	UPVC	13	1:185	8.70	
2	8.68	300	UPVC	36	1:240	8.53	
3	8.51	300	UPVC	16	1:160	8.41	
4	8.34	300	UPVC	14	1:200	8.27	
5	8.26	225	UPVC	1	1:100	8.25	
6	8.25	450x250	RCP	9	1:180	8.20	PUNCH INTO EXISTING CULVERT DOWNSTREAM

PIT SCHEDULE

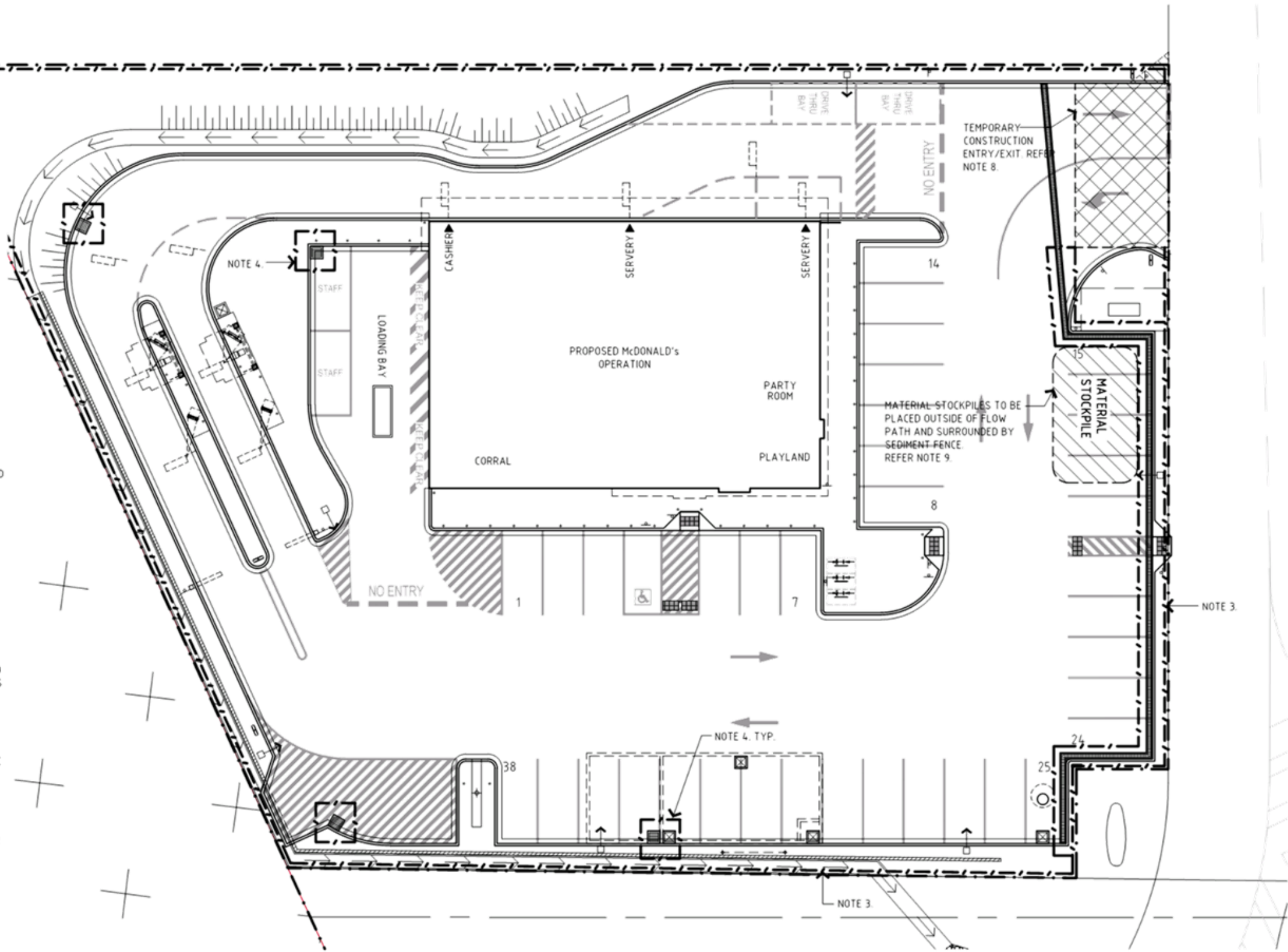
TAG	TYPE	SIZE	RL TOP	INVERT	COVER	NOTES
A	INLET	600 x 600	9.30	8.77	CLASS D GRATE	PROVIDE STORMSACK PIT INSERT
B	INLET	600 x 900	9.70	8.68	CLASS D GRATE	PROVIDE STORMSACK PIT INSERT
C	INLET	600 x 900	9.30	8.51	CLASS D GRATE	PROVIDE STORMSACK PIT INSERT
D	INLET	900 x 900	9.30	8.41	CLASS D GRATE	PROVIDE STORMSACK PIT INSERT
E	TRENCH GRATE	300 mm WIDE	VARIES	8.26	CLASS D HEELSAFE GRATE	IL SPECIFIED FOR OUTLET
F	ECOCEPTOR	1500 SERIES	8.85	8.26	CLASS D SEALED LID	
G	JUNCTION	900 x 900	8.85	8.25	CLASS D SEALED LID	
AP1	ACCESS PIT	900 x 900	9.30	8.34	CLASS D SEALED LID	ACCESS PIT
AP2	ACCESS PIT	900 x 900	9.45	8.34	CLASS D SEALED LID	ACCESS PIT
AP3	ACCESS PIT	900 x 900	9.40	8.34	CLASS D SEALED LID	ACCESS PIT

Revisions	General Notes	Drawing Notes
A DA ISSUE	Do not scale this drawing. The drawing shows design intent only. All dimensions to be checked on site prior to construction or production. Construction details to be confirmed by contractor/manufacturer. This is a computer generated drawing. Do not amend by hand. Figure dimensions are to be used. Contact architect for clarification if dimensions are not clear. All dimensions are in millimeters. All discrepancies and omissions on site must be reported to the architect for their comments or approval prior to commencing work.	

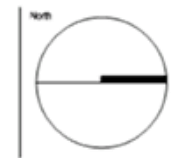
North	 Client: McDonald's Australia Limited AIN 43 058 495 528 02 9875 9959 11 Deakin Way, NSW 2005 Project Manager: 	Architect:  ARCHITECTS, ENGINEERS AND PROJECT MANAGERS 38 Willoughby Place, Crown Hill, NSW 2061 TEL: (02) 9450 9020 FAX: (02) 9439 1224 Consultants:	Project: McDONALD'S RESTAURANT BIOMOD 425 Location: 859-865 PORT WAKEFIELD RD SOLVAY, SA 5110	DA ISSUE NOT FOR USE DURING CONSTRUCTION Scale: N/A Series: N/A Drawing: N/A NOTES AND SCHEDULES Project Number: 190029 Drawing Number: C110 Issue: A
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E & S CONTROL NOTES:

1. BUILDER IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING EROSION AND SEDIMENT CONTROL MEASURES. ALL MEASURES SHALL BE INSPECTED EACH WORKING DAY AND MAINTAINED AS REQUIRED.
2. BUILDER SHALL ENGAGE AN APPROPRIATE QUALIFIED PERSON TO UPDATE THE CONCEPTUAL E&S PLAN TO REFLECT THEIR STAGING AND REQUIREMENTS.
3. BUILDER SHALL PROVIDE SEDIMENT FENCING MATERIAL DURING CONSTRUCTION TO THE LOW SIDE BOUNDARIES. THE SEDIMENT FENCING MATERIAL TO CYCLONE WIRE SECURITY FENCE. SEDIMENT CONTROL FABRIC SHALL BE AN APPROVED MATERIAL (EG. HUMES PROPEX SILT STOP) STANDING 300 ABOVE GROUND AND EXTENDING 150 BELOW GROUND.
4. EXISTING AND NEW DRAINS LOCATED WITHIN THE SITE SHALL TO HAVE DROP INLET GRATE WRAPPED IN APPROPRIATE GEOTEXTILE FABRIC AND PLACED INTO POSITION WITHIN CONCRETE SURROUND.
5. NO PARKING OR STOCKPILING OF MATERIALS IS PERMITTED ON THE LOWER SIDE OF THE SEDIMENT FENCE.
6. GRASS VERGES SHALL BE MAINTAINED AS MUCH AS PRACTICAL TO PROVIDE A BUFFER ZONE TO THE CONSTRUCTION SITE.
7. ROOF DRAINAGE IS TO BE CONNECTED TO THE STORMWATER SYSTEM AS SOON AS PRACTICAL.
8. CONSTRUCTION ENTRY/EXITS SHALL BE AS PER DETAIL. BUILDER SHALL ENSURE ALL DROPPABLE SOIL AND SEDIMENT IS REMOVED PRIOR TO CONSTRUCTION TRAFFIC EXITING SITE. BUILDER SHALL ENSURE ALL CONSTRUCTION TRAFFIC ENTERING AND LEAVING THE SITE DO SO IN A FORWARD DIRECTION.
9. MATERIAL STOCKPILES (EG. SAND OR SOIL) SHALL BE CONTAINED WITHIN A SILT FENCE IN ACCORDANCE WITH SEDIMENT FENCE DETAIL AND PLACED OUTSIDE OF FLOW PATHS OF STORMWATER.
10. BUILDER TO INSTALL GRAVEL BAGS (AS DETAILED) WHERE REQUIRED.



Revisions	General Notes	Drawing Notes
A. DA ISSUE	Do not scale this drawing. The drawing shows design intent only. All dimensions to be checked on site prior to construction or production. Construction details to be confirmed by contractor/manufacturer. This is a computer generated drawing. Do not amend by hand. Figure dimensions are to be used. Contact architect for clarification if dimensions are not clear. All dimensions are in millimeters. All discrepancies and omissions on site must be reported to the architect for their comments or approval prior to commencing work.	
12/08/20 SH GM		
Date	Chk	Iss



Client
 McDonald's Australia Limited
 ADV 43 088 496 528
 02 9870 9068
 11 Deogroup@au.mcd.com

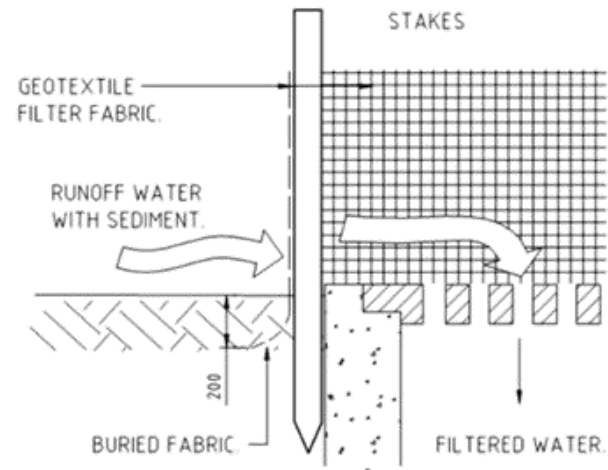
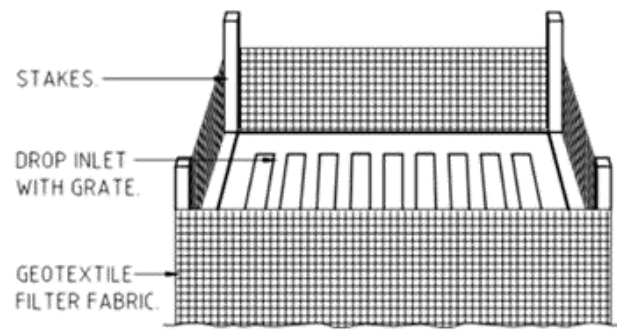
Project Manager
 NATIONAL DEVELOPMENT GROUP

Architect
 Richmond+Ross
 CONSULTING ENGINEERS AND PROJECT MANAGERS
 18 Wroughtley Road, Clovelly NSW 2035
 TEL: (02) 9450 9000 FAX: (02) 9438 1204

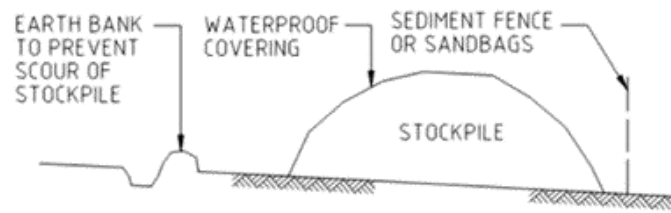
Project
 McDONALD'S RESTAURANT
 BIOMOD 425

Location
 858-866 PORT WAKEFIELD RD
 BULVAR SA 5110

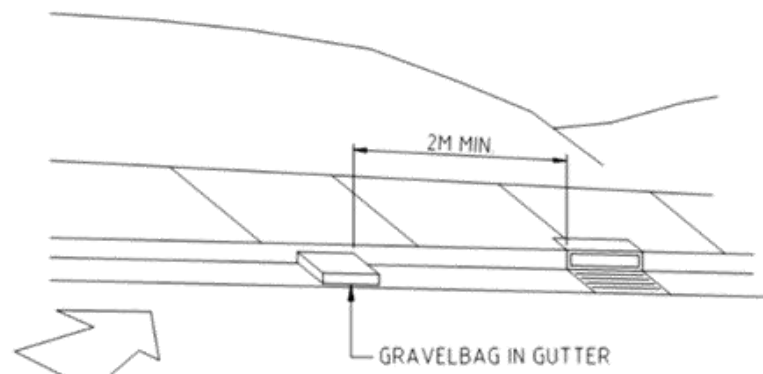
DA ISSUE		
NOT FOR USE DURING CONSTRUCTION		
Scale	Series	
1:250	BIOMOD_425	
Drawing		
SEDIMENTATION PLAN		
Project Number	Drawing Number	Issue
190029	C301	A



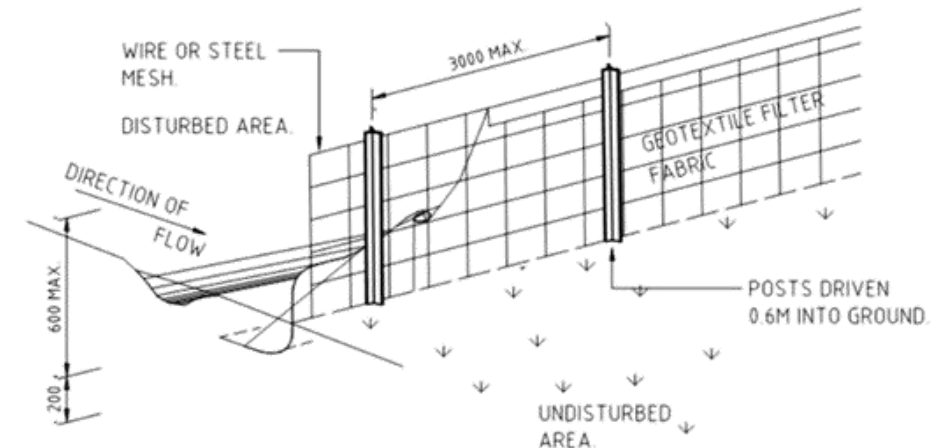
SEDIMENT TRAP TO STORMWATER SUMP



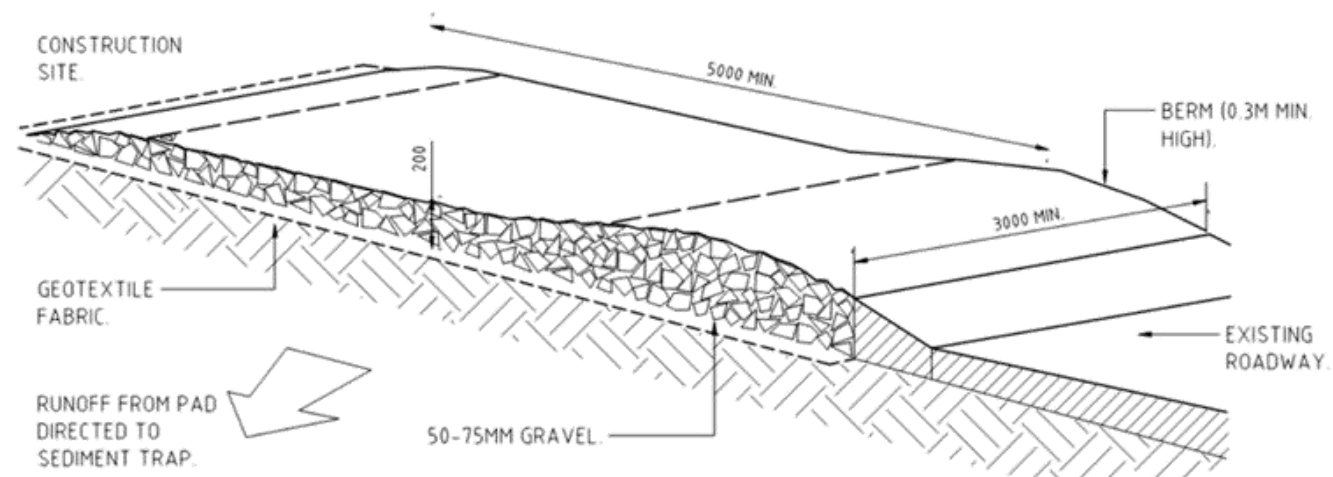
BUILDING MATERIAL STOCKPILES



GRAVELBAG KERB SEDIMENT TRAP



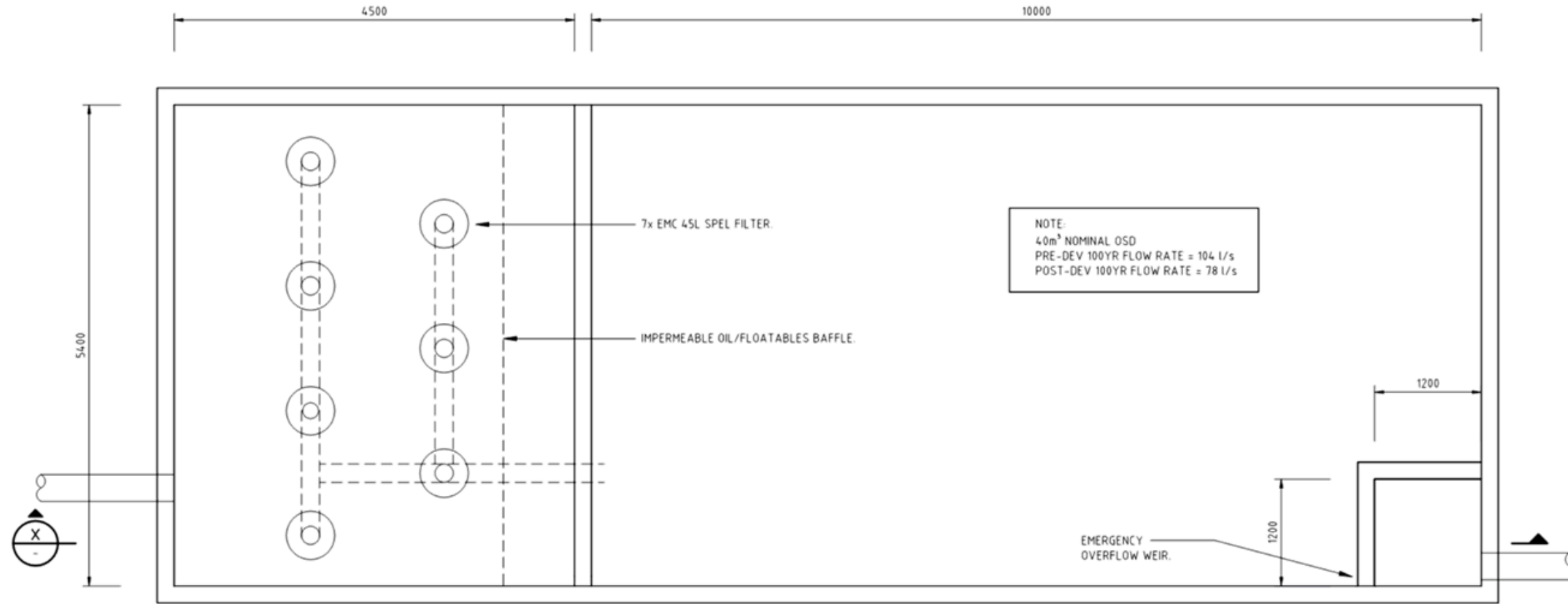
SEDIMENT FENCE



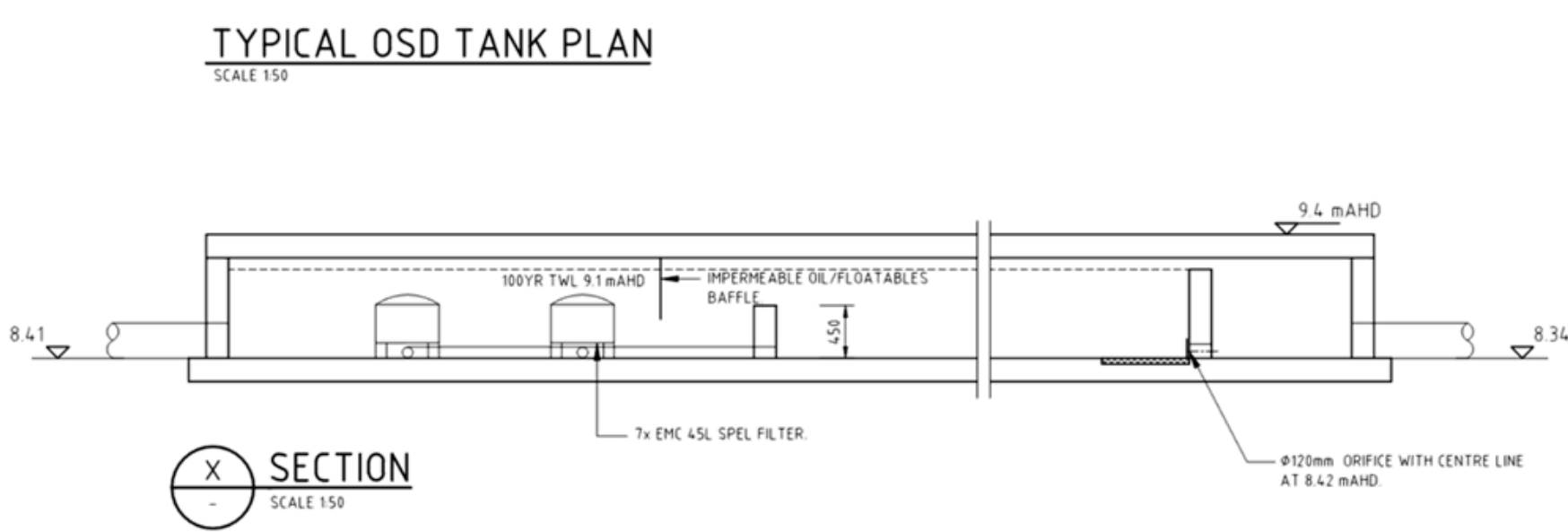
TEMPORARY CONSTRUCTION EXIT

Revisions	General Notes	Drawing Notes
A. DA ISSUE	Do not scale this drawing. This drawing shows design intent only. All dimensions to be checked on site prior to construction or production. Construction details to be confirmed by contractor/manufacturer. This is a computer generated drawing. Do not amend by hand. Figure dimensions are to be used. Contact architect for clarification if dimensions are not clear. All dimensions are in millimeters. All discrepancies and omissions on site must be reported to the architect for their comments or approval prior to commencing work.	
25/08/20	SH JK	
Date	Chk. Int.	

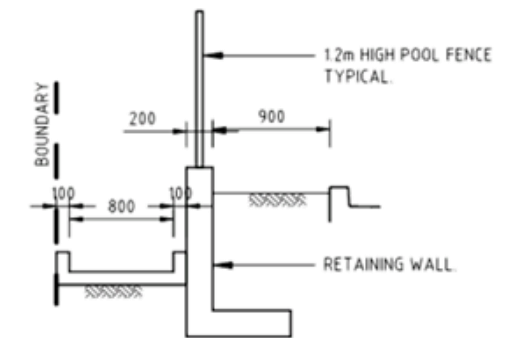
North Client: McDonald's Australia Limited A/N: 43 056 496 526 T: 02 9870 9909 T: 02 9499 9900 T: 02 9499 9900 Project Manager: NATIONAL DEVELOPMENT GROUP	Architect: Richmond+Ross CONSULTING ARCHITECTS AND PROJECT LEADERS 38 Willoughby Road, Crown Hill NSW 2061 TEL: (02) 9499 9900 FAX: (02) 9499 1204 Consultants:	Project: MCDONALD'S RESTAURANT BIOMOD 425 Location: 155-165 PORT WAKEFIELD RD BULVAR, SA 5110	DA ISSUE NOT FOR USE DURING CONSTRUCTION
			Scale: N/A Series: N/A Drawing: EROSION & SEDIM. CONTROL DETAILS Project Number: 190029 Drawing Number: C310 Issue: A



TYPICAL OSD TANK PLAN
SCALE 1:50



SECTION X-X
SCALE 1:50



TYP. UPSTREAM SECTION OVERLAND FLOW DIVERSION CHANNEL
SCALE 1:50
1:350 (MIN.) FALL

Revisions	Revised Notes	Drawn by/checked by
B DA ISSUE	07.12.20 SH	
A DA ISSUE	25.09.20 SH JK	
No. of issues	Date	Checked by

Not

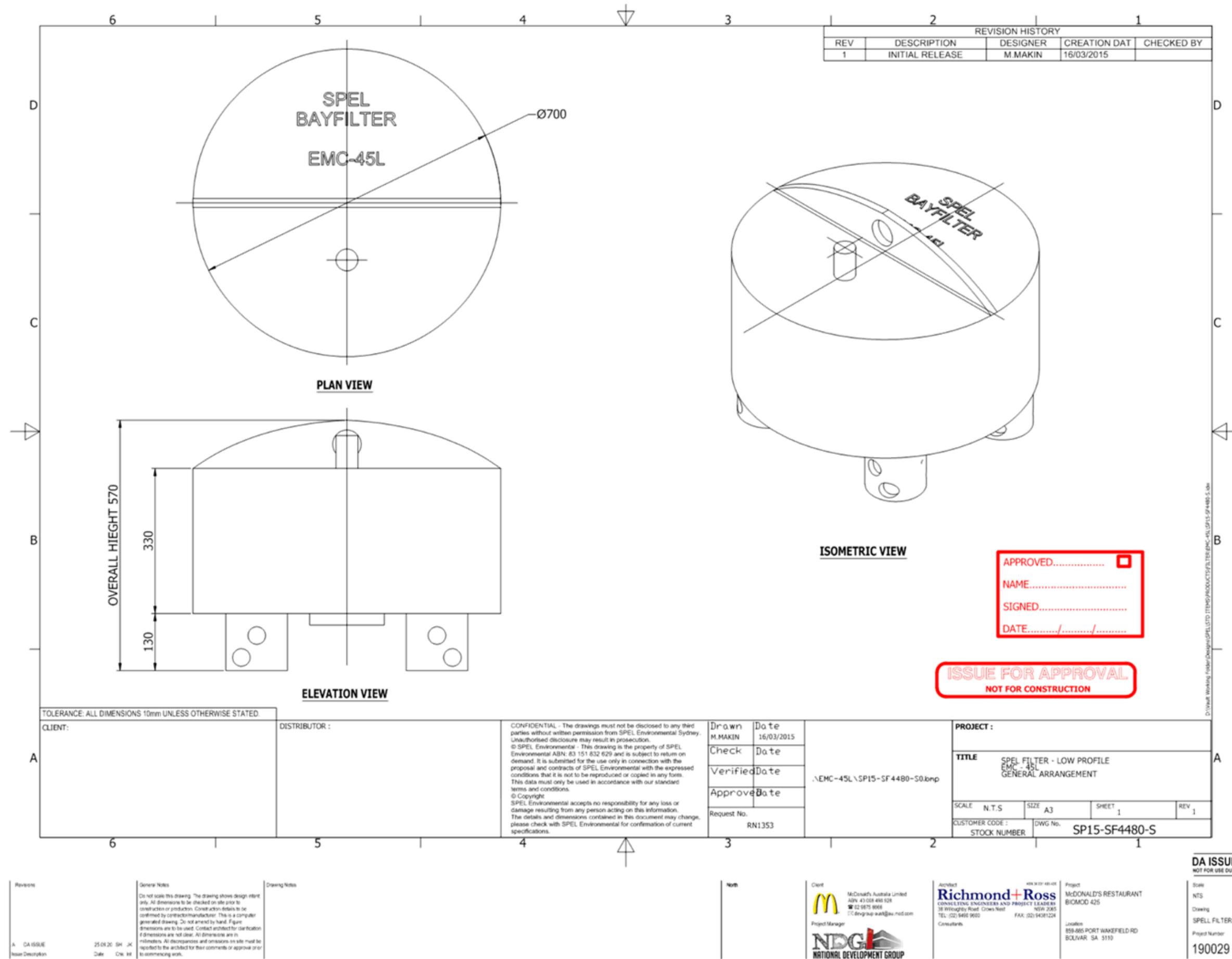
Client: McDonald's Australia Limited
 ADN 43 008 490 528
 02 9870 8008
 17 Design | 18@mc.com.au

Prepared by: NDCG
NATIONAL DEVELOPMENT GROUP

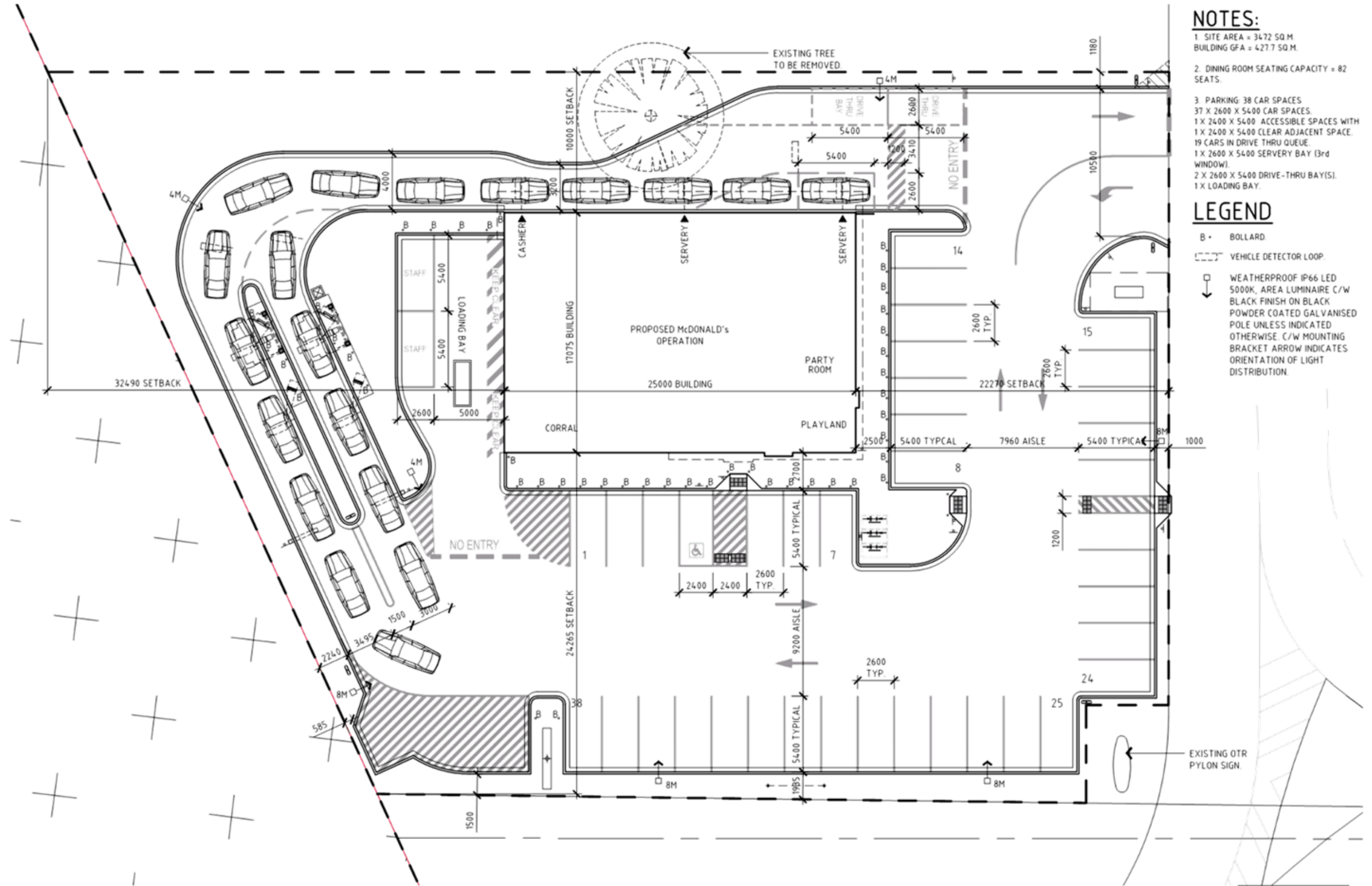
Acc. No: Richmond+Ross
 CONSULTING ENGINEERS AND PROJECT LEADERS
 18 Wills St, Geelong VIC 3220
 TEL: (03) 5450 9600 | A: (03) 54381222

Project: McD NALD'S RESTAURANT
 BI M D 425
 Location: 855 P. RT WAKEFIELD RD
 B LVAR SA 5110

DA ISSUE		NOT FOR USE DURING CONSTRUCTION	
Scale	Series		
1:50			
Drawn by	SD TANK PLAN AND SECTION		
Project number	Drawn by number	Issue number	Issue date
190029	C350	B	



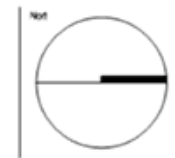
Item 8.1.1 - Attachment 1 - Proposal Plans and Supporting Documentation including Statement of Support and Statement of Effect



- NOTES:**
1. SITE AREA = 3472 SQ M
BUILDING GFA = 427.7 SQ M.
 2. DINING ROOM SEATING CAPACITY = 82 SEATS.
 3. PARKING 38 CAR SPACES
37 X 2600 X 5400 CAR SPACES.
1 X 2400 X 5400 ACCESSIBLE SPACES WITH
1 X 2400 X 5400 CLEAR ADJACENT SPACE.
19 CARS IN DRIVE THRU QUEUE.
1 X 2600 X 5400 SERVERY BAY (3rd WINDOW).
2 X 2600 X 5400 DRIVE-THRU BAY(S).
1 X LOADING BAY.

- LEGEND**
- B • BOLLARD
 - - - - VEHICLE DETECTOR LOOP.
 - ☐ WEATHERPROOF IP66 LED 5000K, AREA LUMINAIRE C/W BLACK FINISH ON BLACK POWDER COATED GALVANISED POLE UNLESS INDICATED OTHERWISE C/W MOUNTING BRACKET ARROW INDICATES ORIENTATION OF LIGHT DISTRIBUTION.

Revisions	Issue Notes	Date	By
B	DA ISSUE	17.08.20 SH	M
A	DA ISSUE	12.08.20 SH	M
No.	Issue Note	Date	By



Client
 McDonald's, a Division of
 ADV 43 008 496 528
 02 9870 9008
 17 Design 4 58 24 mcd.com
 Project Manager
 NATIONAL DEVELOPMENT GROUP

Architect
 Richmond+Ross
 CONSULTING ENGINEERS AND PROJECT MANAGERS
 18 Wills St, Rydalmere NSW 2108
 TEL: (02) 9450 9000 A: (02) 94311212
 Core Hours

Project
 McD NALD'S RESTAURANT
 BI M D 425
 Location
 855 P RYDALMERE RD
 B LVAR SA 5110


DA ISSUE			
NOT	OR USE DURN	CONSTRUCTION	
Scale	Series		
1:250	BI M D 425		
Drawn By			
SITE PLAN			
Project Number	Drawing Number	Issue	
190029	A062	B	

RESPONSE TO DIT RFI:

Proposed McDonalds
Restaurant
Bolivar (adjacent OTR)

REF: DA361/778/20

15 December 2020 S184940

 GTA consultants

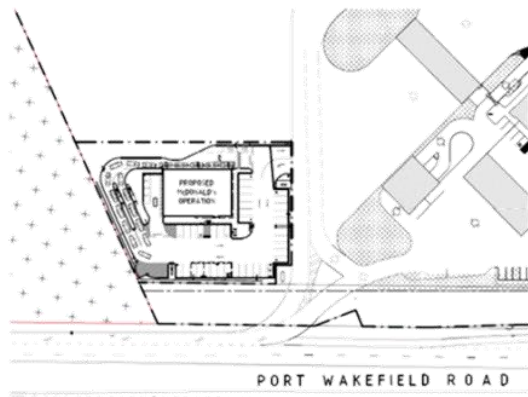
Item 8.1.1 - Attachment 1 - Proposal Plans and Supporting Documentation including Statement of Support and Statement of Effect

Introduction

A Development Application (DA 361/778/20) has been lodged for a proposed McDonalds restaurant to be located adjacent to the existing OTR Bolivar at 859-885 Port Wakefield Road in Bolivar.

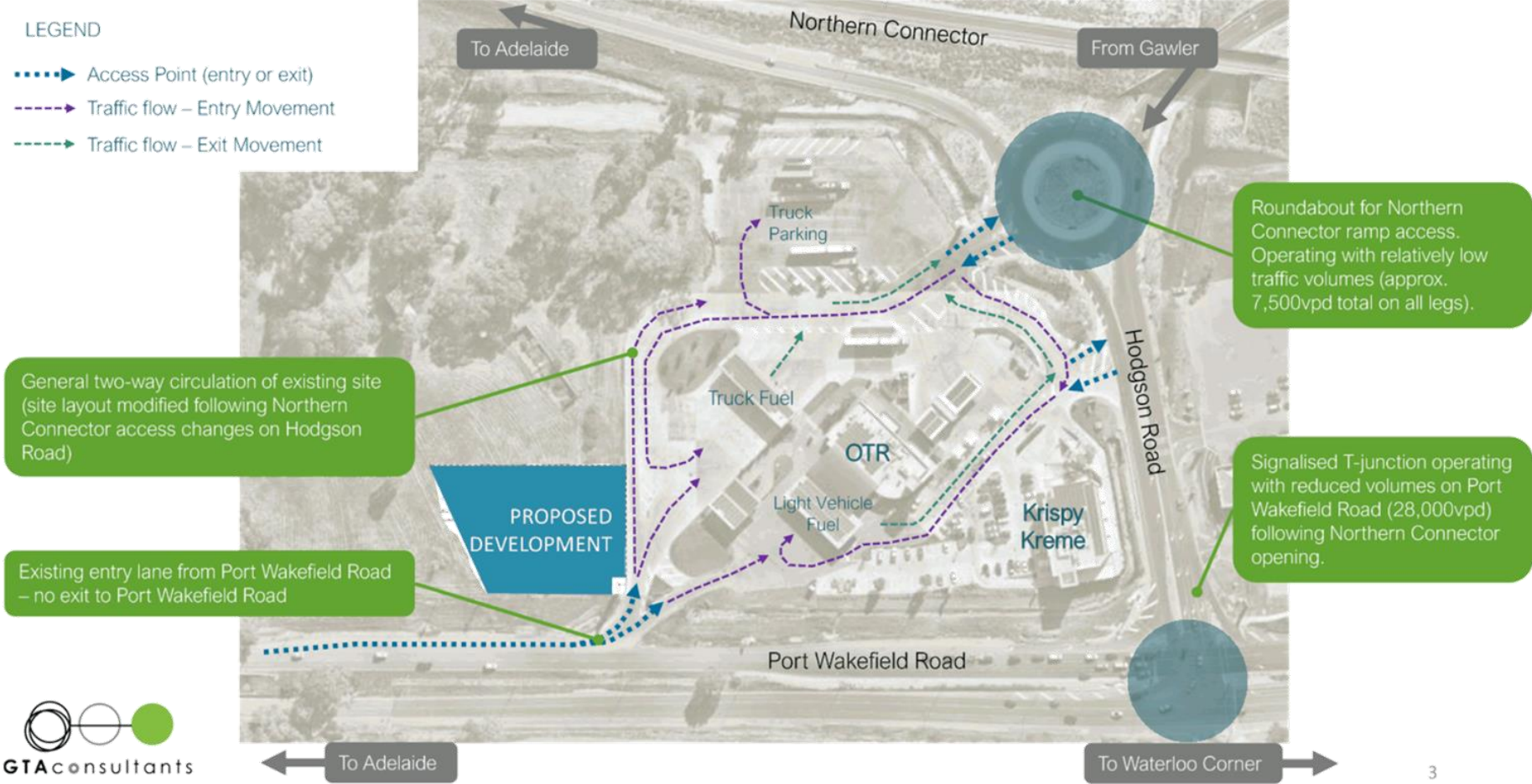
The proposed development will comprise a McDonalds restaurant with car park and access point adjacent to the existing OTR Bolivar. This site permits two-way movements to and from the proposed development and Hodgson Road, and direct entry from Port Wakefield Road.

This report has been prepared to provide information requested by Department for Infrastructure and Transport in the Further Information Request dated 17 November 2020.



- The Department for Infrastructure and Transport has sought further information as follows:
- *Details the traffic generation of the subject development and overall development;*
 - *Details the composition and distribution of traffic accessing the subject site;*
 - *Provides an assessment of the traffic impact of the subject development on the access points to the site and the adjacent Port Wakefield Road/Hodgson Road junction;*
 - *Demonstrates that the proposed access to the internal driveway will not result in queueing of traffic back to Port Wakefield Road, particularly during peak times;*
 - *Details how traffic exiting the subject development and wishing to access the adjacent service station will be directed through the site to avoid conflict adjacent the Port Wakefield Road access;*
 - *Details of how traffic exiting the subject development and site will be directed away from the Port Wakefield Road access;*
 - *Details any changes required to the access points, existing line marking or way finding signage adjacent to the Port Wakefield Road access to facilitate safe access.*

1. Existing Site Context



Item 8.1.1 - Attachment 1 - Proposal Plans and Supporting Documentation including Statement of Support and Statement of Effect

2. Traffic Demands for Proposed Development

McDonalds Typical Traffic Generation:
 Daily: 2,000 vpd
 PM Peak Hour: 150 vph
 Drive-Thru typically 70% of total traffic

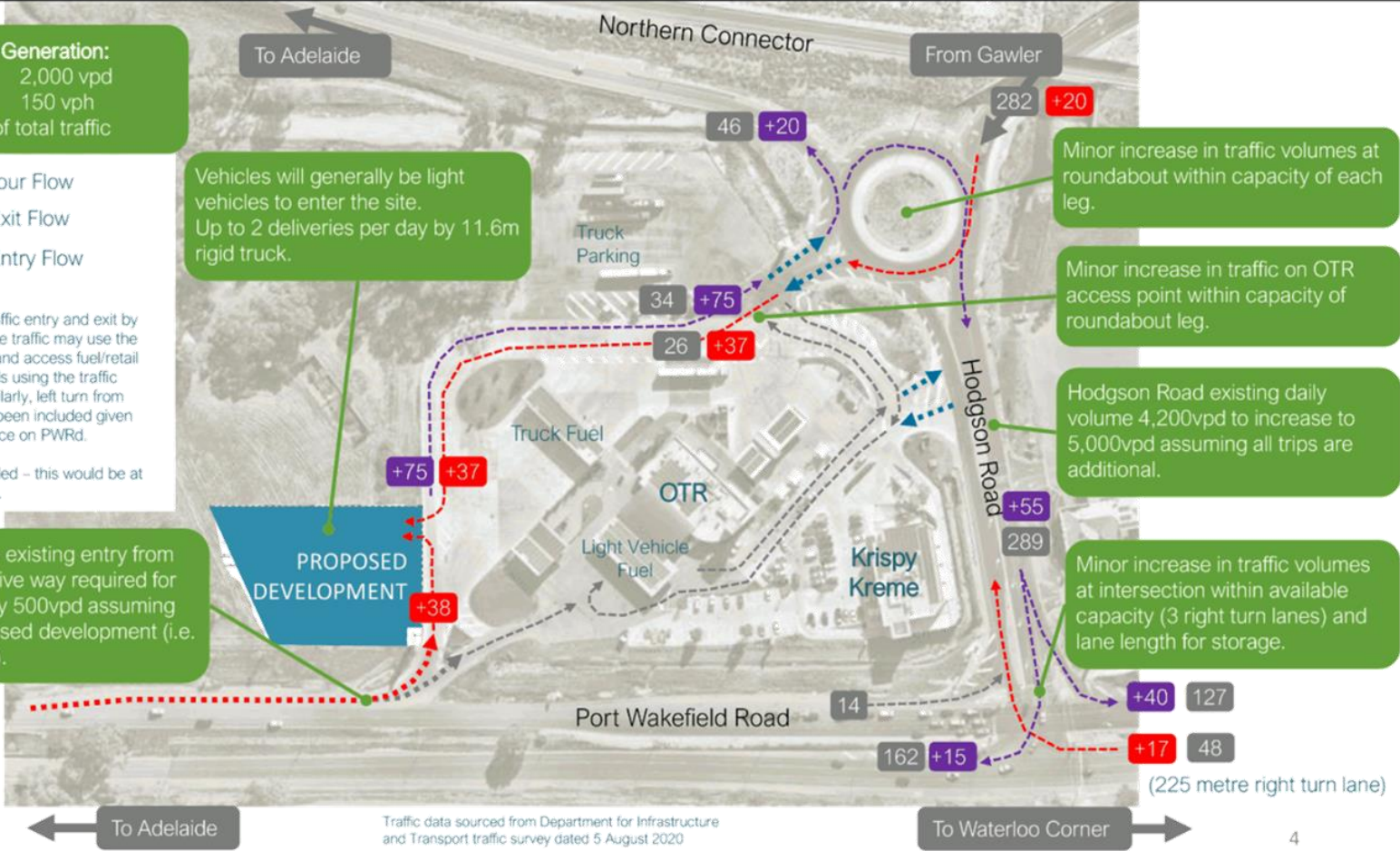
- 34 Existing PM Peak Hour Flow
- +75 Predicted Change Exit Flow
- +37 Predicted Change Entry Flow

NOTE:

1. This assessment has assumed traffic entry and exit by the two main access points. Some traffic may use the secondary Hodgson Road traffic and access fuel/retail services before or after McDonalds using the traffic routes shown within the site. Similarly, left turn from PWRd to Hodgson Road has not been included given likely low volume and main entrance on PWRd.
2. Passing trade has not been included – this would be at least 35% of traffic in this location.

Minor increase in traffic on existing entry from Port Wakefield Road. No give way required for entering traffic. Increase by 500vpd assuming all additional trips to proposed development (i.e. no passing trade discount).

Vehicles will generally be light vehicles to enter the site. Up to 2 deliveries per day by 11.6m rigid truck.



Traffic data sourced from Department for Infrastructure and Transport traffic survey dated 5 August 2020

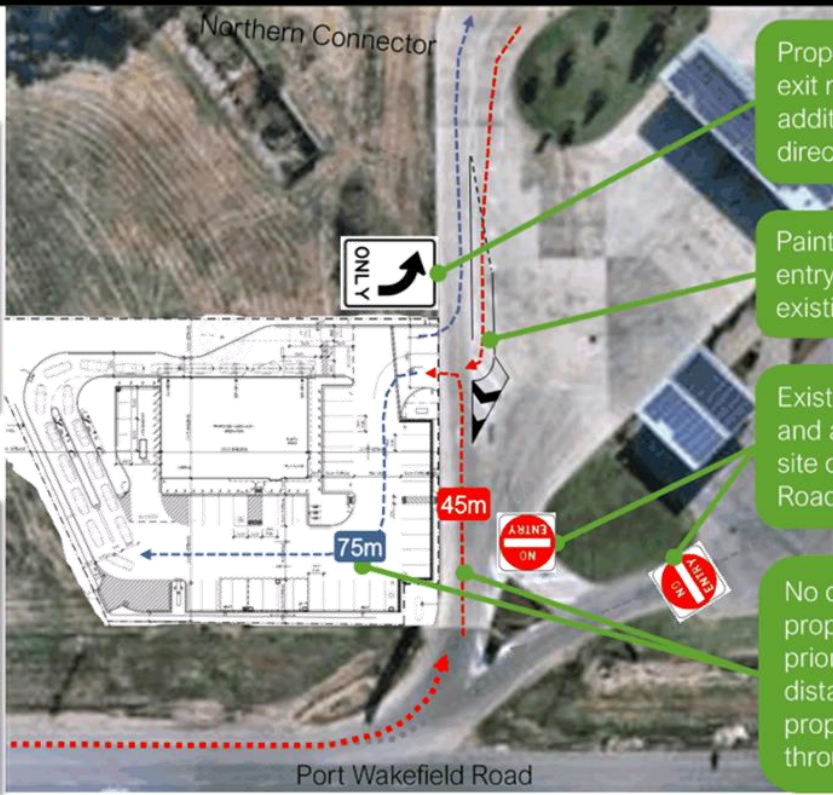
Item 8.1.1 - Attachment 1 - Proposal Plans and Supporting Documentation including Statement of Support and Statement of Effect

3. Traffic Management for Access

Photo 1: View east (site on right) of existing painted median and No Entry signage to prevent exit to Port Wakefield Road



Photo 2: View south existing light vehicle fuel entry with No Entry signs to exit to Port Wakefield Road and proposed development



Proposed "All Traffic Turn" (left) signage for exit movements at proposed access point, in addition to McDonalds style signage for exit direction (to left).

Painted right turn lane proposed to define entry movement from west and integrate with existing painted island/pavement bar median.

Existing "No Entry" signage located to east, and also south of light vehicle fuel area, within site clearly defines no exit to Port Wakefield Road. See photo 1 and 2.

No queuing anticipated as vehicles entering proposed access point from PWRd have priority over all other movements. Adequate distance will be provided from PWRd entry to proposed access point, and to internal drive-through entry point (45 metres + 75 metres).



← To Adelaide

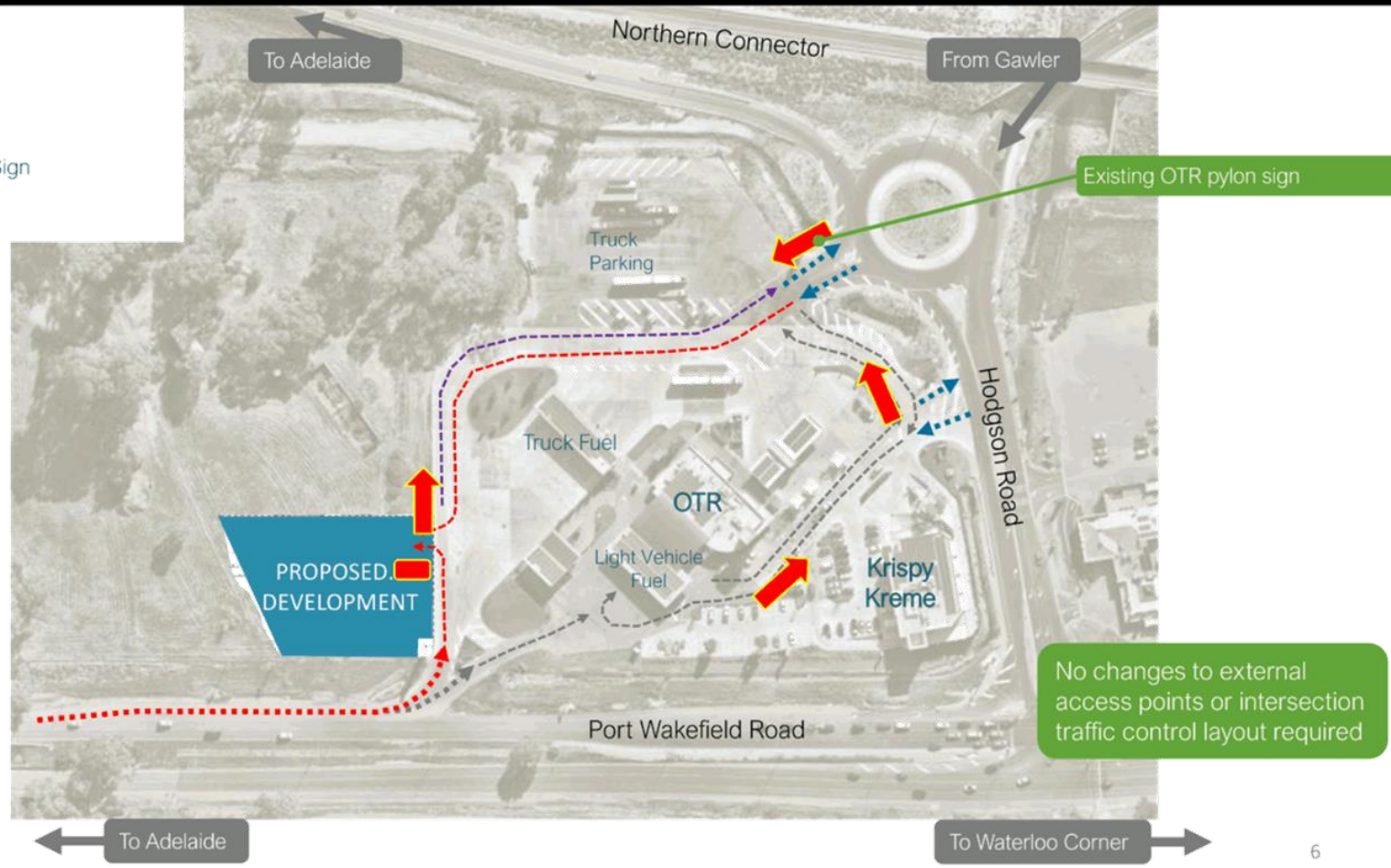
Traffic data sourced from Department for Infrastructure and Transport traffic survey dated 5 August 2020

To Waterloo Corner →

Item 8.1.1 - Attachment 1 - Proposal Plans and Supporting Documentation including Statement of Support and Statement of Effect

4. Wayfinding

- LEGEND
- ➔ Wayfinding sign
 - McDonalds Pylon Sign



Item 8.1.1 - Attachment 1 - Proposal Plans and Supporting Documentation including Statement of Support and Statement of Effect

5. Traffic Assessment Summary

1

Existing Site Context:
The proposed development will comprise a McDonalds restaurant with car park and access point adjacent to the existing OTR Bolivar. The OTR site permits two-way movements to and from the proposed development and Hodgson Road, and direct entry from Port Wakefield Road.

2

Traffic Assessment:
Proposed development will have minor impact on adjacent road network, intersections and access points for the OTR site. Approximately 150 vehicles per hour anticipated and approximately 2,000 vehicles per day.
A high level of passing trade would be expected however has not been assumed in the analysis which indicates minor change in traffic volumes on key movements at Hodgson Road and Port Wakefield Road intersection.
There is available capacity in the adjacent road network for the proposed development, even assuming no passing trade discounts. The total daily and peak hour traffic generation of the site is minor and well within spare capacity given the existing traffic volumes on Port Wakefield Road, Northern Connector ramps and Hodgson Road.

3

Traffic Management for Access:
Access will be available for entry from Port Wakefield Road, and entry/exit from Hodgson Road. A right turn lane will be installed adjacent the proposed internal access point to assist in directing traffic to the site safely and efficiently.
The proposed development will operate appropriately within the existing two-way access movements around the OTR site, which are clearly sign-posted to delineate not-permitted movements back to Port Wakefield Road.
There is appropriate separation of the proposed access point from the Port Wakefield Road entry which, in conjunction with priority of this movement at the proposed access point, will not result in any queuing back to Port Wakefield Road.
Wayfinding signs will be installed at key locations within the OTR site to ensure access to and from the proposed development.



Karyn Brown

From: Dean Nicolle <dn@dn.com.au>
Sent: Wednesday, 13 May 2020 10:56 AM
To: Smith Mark
Cc: Marriot-Smith Laura; David Hutchison
Subject: Re: Bolivar McDonald's

Hi Mark,

As I relayed to Laura Marriot-Smith by phone, I inspected the site on the 6th May 2020. The subject tree is a peppercorn tree (*Schinus molle*), as indicated on the plans that you had attached.

Schinus molle is a species that is exotic to Australia and as such the tree is not protected by the *Native Vegetation Act 1991* (as also indicated by David Hutchison). The species is also exempt from the *Development Act 1993*.

Let me know if you require any further information regarding the tree.

Regards,
Dean.

Dean Nicolle

*Currency Creek Arboretum
Arboriculture - Botany - Ecology - Eucalypt Research*

PO Box 808
Melrose Park SA 5039 AUSTRALIA

T: +61 413 214 303
E: dn@dn.com.au
W: www.dn.com.au

On 13 May 2020, at 10:24 am, Smith Mark <Mark.Smith@au.mcd.com> wrote:

Hi Dean/David,

Please see attached all relevant information from OTR in relation to native veg and their application.

Are you able to review and let us know if any issues as a whole or in relation to removing that tree.

Kind Regards,

Mark Smith | Development Manager Construction | McDonald's Australia Ltd
2 Smith Street, Collingwood Victoria 3066
0421 594 542 | * mark.smith@au.mcd.com
<image001.png>

ü Please consider the environment when choosing to print this email

From: Kim Pomario <K.Pomario@pcinfrastructure.com.au>

1

Sent: Wednesday, 13 May 2020 10:44 AM
To: Marriot-Smith Laura <Laura.Marriot-Smith@au.mcd.com>
Cc: Smith Mark <Mark.Smith@au.mcd.com>
Subject: RE: Bolivar McDonald's

Hi Laura,

I have attached a copy of our original report that was heavily contested by DEWNR. I have also attached a copy of the approved tree removal plan.

Regards

Kim

From: Marriot-Smith Laura <Laura.Marriot-Smith@au.mcd.com>
Sent: Wednesday, May 13, 2020 9:39 AM
To: Kim Pomario <K.Pomario@pcinfrastructure.com.au>
Cc: Smith Mark <Mark.Smith@au.mcd.com>
Subject: RE: Bolivar McDonald's

Hi Kim,

Hope the week is treating you well.

Just chasing up any information/documents on the native veg as we will likely apply to remove the tree and redesign but don't want to give the go ahead until we have all the info.

Kind regards,

Laura Marriot-Smith | Development Manager – Real Estate | McDonald's Australia Ltd
2 Smith Street, Collingwood Victoria 3066
(M 0424 450 225 | * laura.marriot-smith@au.mcd.com
<image001.png>

ü Please consider the environment when choosing to print this email

From: Marriot-Smith Laura
Sent: Tuesday, 5 May 2020 2:21 PM
To: Kim Pomario <K.Pomario@pcinfrastructure.com.au>
Cc: Smith Mark <Mark.Smith@au.mcd.com>
Subject: RE: Bolivar McDonald's

Thanks for this Kim.

Should be enough to get started!

By way of update, I am arranging for our arborist to head out to site tomorrow to check our the trees on site as I know there are issues with native vegetation.

I had been in contact with Andrew Casper to ask if he had a copy of any arborist reports in the meantime.

I will keep you posted on this as may effect our overall design.

Kind regards,

Laura Marriot-Smith | Development Manager – Real Estate | McDonald's Australia Ltd
2 Smith Street, Collingwood Victoria 3066
(M 0424 450 225 | * laura.marriot-smith@au.mcd.com
<image001.png>

ü Please consider the environment when choosing to print this email

From: Kim Pomario <K.Pomario@pcinfrastructure.com.au>
Sent: Tuesday, 5 May 2020 2:01 PM
To: Marriot-Smith Laura <Laura.Marriot-Smith@au.mcd.com>
Cc: Smith Mark <Mark.Smith@au.mcd.com>
Subject: RE: Bolivar McDonald's

Good afternoon Laura,

I have attached the civil construction documents for the OTR. The “as built” may vary to the attached. This will however give your team a very good idea of the required levels. The attached plans also do not show the new entry off the Northern Connector at the NW corner of the site.

I do not have the attached in cad, only PDF.

Please do not hesitate to contact me should you require any additional information with regards to the above.

Regards

Kim

From: Marriot-Smith Laura <Laura.Marriot-Smith@au.mcd.com>
Sent: Monday, May 4, 2020 2:42 PM
To: Kim Pomario <K.Pomario@pcinfrastructure.com.au>
Cc: Smith Mark <Mark.Smith@au.mcd.com>
Subject: Bolivar McDonald's

Hi Kim,

Hope you are well.

We are in the middle of preparing our DA for the Bolivar site.

I was wondering if you would be able to share any of the below to help with our design and application (initial layout attached);

- Engineering drawings for OTR from DA
- Any Site Survey’s prepared that would could gage levels from etc

Any information would really help with our design.

If unavailable I will need to organise for a surveyor to go to site and prepare a report – is there anyone I would need to touch base with before sending him?

Kind regards,

Laura Marriot-Smith | Development Manager – Real Estate | McDonald's Australia Ltd

2 Smith Street, Collingwood Victoria 3066
(M 0424 450 225 | * laura.marriot-smith@au.mcd.com
<image001.png>

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<Appendix [D.Report.Site Plan.pdf](#)><Report.NativeVegetation.20140630.pdf><AppendixB.Report.Tree Locations.pdf><2014_2022_Parade Systems_NV_Advisory.pdf>



26th May 2020

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Chief Executive Officer
City of Salisbury
PO Box 8
SALISBURY SA 5108

Access Planning (SA) Pty Ltd
ABN 57 089 702 241

235 Henley Beach Road
Torrensville SA 5031

Telephone 08 8130 7222
Facsimile 08 8130 7299
admin@accessplanning.com.au

www.accessplanning.com.au

Attention: Mr. Aaron Curtis

Dear Aaron,

RE: PROPOSED RESTAURANT (INLCUDING ASSOCIATED DRIVE-THROUGH SERVICE, CAR PARKING AND SIGNAGE) – 859 PORT WAKEFIELD ROAD, BOLIVAR

Please find herewith a completed Development Application form, draft proposal plans, electricity declaration and a Certificate of Title for the development of a shop (McDonald's) at the above address.

The land is located substantially in the Primary Production zone and partly within the Open Space zone, Policy Area 15.

As the site straddles the zone boundary, both sets of zone provisions are required to be taken into consideration in an assessment of the development.

Either way a shop is listed as being a non-complying development in both zones subject to some floor area limitations, which in this instance are exceeded for both zones.

Additionally, an advertisement is listed as being a non-complying development as the land is adjacent to a road with posted speed limit in excess of 80kph, noting that the sign area exceeds the exceptions otherwise applicable to make this aspect of the land use a consent activity.

The plans and information accompanying this application are draft plans only, and whilst they have been prepared with a reasonable amount of detail, they are subject to alteration pending more informed discussions with Council.

McDonald's is aware of the stormwater drainage issues in the locality and myself and representatives of McDonald's would be happy to meet with you post assessment of this preliminary submission to discuss and to seek to resolve any issues which you may consider require attention.

Should the Council resolve to proceed with an assessment of the application, additional information pertaining to lighting, stormwater management and flooding and traffic will be provided, and will inform any possible changes to the site layout/development. In addition, a detailed landscaping plan will form part of the final plans provided to Council for assessment.



STATEMENT IN SUPPORT

The proposal, together with an assessment summary of the development, is outlined below. This correspondence should be considered as a Statement in Support of the proposal as required by Regulation 17 (1) of the Development Regulations, 1993.

SUBJECT LAND AND LOCALITY

The subject land is situated at the intersection of Hodgson Road and Port Wakefield Road Bolivar. Hodgson Road is a link road between Port Wakefield Road and Northern Expressway (NEXY) and provides access to the site via the On the Run development which occupies the northern part of the land.

As indicated above, land has recently been developed with a substantial Integrated service station development, stand alone retail facilities and extensive signage. Traffic conditions in the locality have similarly been significantly modified as a consequence of works associated with the NEXY

The land is legally identified in Allotment 25 in Deposited Plan 85405 and is more particularly described in Certificate of Title Register Book Volume 6169, Folio 78; a copy of which is included with the application documents.

Port Wakefield Road is identified as a controlled access road, although the status of this is likely to change now the NEXY is diverting a large amount of traffic from Pt Wakefield Road.

There is an easement along the Port Wakefield road frontage of the land to the Natural Gas Authority of SA. The easement is shown on the proposal plans. The development does impact on the easement.

A second easement registered on the title is located some distance from the development site and is not impacted by it.

The land abuts the alignment of the NEXY to the west, Port Wakefield Road, then largely open space to the east, open space to the south and to the north the premises of the White Horse Inn Hotel.

Port Wakefield Road is identified as a primary arterial road. Hodgson Road is a secondary arterial Road. There is no meaningful current traffic data for these roads at present.

The subject land is shown in more detail in the Figures 1 and 2 below.



Figure 1: Subject land and immediate locality

Source: SAAPA



Figure 2: Updated image of site and locality showing completed works on the NEXY Source: Nearmap

3.0 THE PROPOSAL

The proposal involves the establishment of a restaurant with an integrated drive through, car parking, landscaping and signage as described below.

Building

Construction of a single storey contemporary designed restaurant/café/take away and drive through takeaway facility to be located at the southern end of the land.

The proposed building has a floor area of 400m² accommodating the dining room, cafe and serving facilities, kitchen and 'back of house' storage and staff facilities/amenities, patron amenities, service area (corral) and a small play-land area for younger children. Two serveries are located in the western elevation of the building to serve patrons of the drive through.

The building adopts a simple form comprising a 6.075m high parapet which screens the roof and roof mounted plant, with an elevated, glazed feature corner rising 5m which accommodates the play facilities in the playland area and allows extensive access to natural light to this aspect of the building.

Significant articulation is provided to the building by way the use of a variety of building material and colour. Vertical and horizontal projections and the use of shade canopies and window placement/building openings highlight various aspects of the land use, such as defining service points, building entries and windows. Signage is also incorporated as a design feature and is coordinated throughout the development.

The building exhibits a substantial setback of some 36m from Port Wakefield Road, being 24.26m inside the High-Pressure gas pipeline easement.

Access/loading bay/car parking/bicycle parking

Access to the development will be by way of existing crossovers presently servicing the OTR development which will provide entry into the one-way drive through lane and an entry and exit to and from the main car park back on to the OTR site thence to the adjoining road network.

Off-street parking is provided for 39 vehicles, including, two stacked staff car parks, one disabled persons car park and two waiting bays. The access ways and parking will be designed and constructed to meet current AS 2890.1.

A loading bay is located adjacent to the southern elevation of the proposed building. This loading bay is located and designed to allow a semi-trailer (up to 15.5 metres in length) to enter and exit the site in a forward direction.

Trading Hours

The proposed hours of operation of the facility will be 24 hours per day, seven days of the week.

Signage/Flagpoles

There will be a range of signs fixed to the building in accord with the high standard of McDonald's corporate signage. They will include:

- A freestanding pylon sign located adjacent to the Port Wakefield Road adjacent the entry to the land. This sign will be ten (12) metres in height, internally illuminated and will feature corporate signage (McDonald's "Big M" and "McCafe" and an information sign advising of the opening times of the facility);
- Ancillary signs including wall signs, directional signs, menu board signs, speed signs, and pedestrian access signs are also proposed.

Waste Management

Commercial waste management standards will apply for this facility with two commercial bins located in the Corral area. McDonald's restaurant staff will conduct litter patrols to keep the restaurant grounds and surrounds clean.

Fencing and Landscaping

The existing rural post and wire fencing is to be maintained to the Port Wakefield Road frontage and to the southern property boundary, the site will otherwise not be fenced.

Extensive landscaping is to be located around the boundary of the site boundary with landscaping beds varying from 1m to 15m wide.

A detailed landscape plan and will be lodged with Council on progress of the application.

McDonald's approach to landscaping is for a high standard of planting (using mature stock for feature trees) and regular ongoing maintenance e. Water sensitive design is a feature of McDonald's landscaping philosophy.

There is a garden store in the corral for storage of garden tools and materials.

5.0 SUPPORTING STATEMENT

The subject land is situated within the Primary Production and Open Space zone, the latter falling also into a policy area (15) as identified on Map Sal/31 of the Salisbury Council Development Plan, consolidated 4 April 2019.



Figure 3: Zone and policy areas affecting the subject land

As mentioned above both a shop and certain forms of advertising are listed as being non-complying forms of development in both zones in which the subject land falls.

Notwithstanding the above, the proposal is considered to represent an appropriate form of development for the following reasons:

- It is highly unlikely the subject site will ever be contemplated for development in accord with the (main), Primary Production zoning of the land. This is reinforced in my view by the recent development of the OTR service station and associated retail development.
- The incorporation of part of the land into the Open Space zone is unusual, as in normal circumstances zone boundaries follow cadastre. I do note however that the boundary is defined by dimensions which would appear to indicate that was a deliberate act. In the absence of any evidence to the contrary it is unusual to include private land in an open space zone but I note that for the most part, the development proposed that falls within the Open Space zone is low is scale, small in mass and mostly comprises driveway and landscaping. Open fencing will maintain an outlook into the Open Space zone such the impact of the development on this zone will be minimal.
- The development has been designed and sited having regard to the nature and scale of the adjoining OTR development and will add to the range of features being provided to what is now, to all intents and purposes, a highway rest stop.
- Although the Development Plan seeks to aggregate shopping facilities in Centre Zones, Convenience restaurants such as McDonald's are a destination in their own right and do not form part of the day to day shopping needs of the community that necessitate a Centre Zone location;
- Convenience restaurants are traditionally sited on arterial roads, from which they draw a large percentage (60%) of their custom, the area required to accommodate the drive through lanes and service access limits their ability to be sited in smaller centre zones, and limited access to arterial roads from larger centres militates against their location in larger, established centres;
- In Centre zones, Convenience restaurants are most often located on stand-alone sites and are not integrated with the principal shopping outlets, reflecting their status as stand-alone developments;
- The hours of operation of the proposed store differ significantly from normal retail outlets (i.e. 24 hours) with peak evening trade occurring at times when other retail outlets are often shut; such that any benefits deriving from the customers of the Convenience restaurant also shopping at other retail outlets in a Centre are minimal;
- Given that the customer base (primarily drive thru and take away orders) of, and reason to visit a Convenience restaurant differs from the day to day shopping trip normally associated with the reason to visit a shopping centre, it is not considered that a location outside of a

designated Centre will detract from the function or result in the deterioration of nearby centre zones through the erosion of the customer base;

- The subject land is very isolated from nearby residential land and is not affected by sensitive neighbouring land uses that might be impacted by the activities and hours of operation resulting from the development.
- The overall height, bulk and scale of the proposed restaurant building will be sympathetic to existing scale and form of existing commercial buildings in the locality and are compatible with the emerging commercial character of development resulting from the recent development of the OTR service station and longer standing White Horse Inn Hotel development;
- The proposal will be constructed from quality materials and together with proposed landscaping will significantly enhance the appearance and visual amenity of the subject land and locality;
- Access to the site will be taken via already approved crossovers and driveways from the adjoining road network;
- It is expected that the site will have the following traffic and car parking characteristics (note: these will be subject to detailed assessment by a Traffic Engineer):
 - The proposed restaurant will generate in the order of 921 vehicle movements per day;
 - The amount of seating is estimated to be approximately 100 in total. Therefore, the 39 car parking spaces proposed exceeds the required 30 parking spaces prescribed in Table Sa/2 relating to off street parking requirements for a restaurant;
 - The car parking and access layout has been designed in accordance with the relevant Australian Standards;
 - The proposed loading/delivery area has been designed to allow a semi-trailer to enter and exit the site (and more particularly, the land) in a forward direction;
- The 12m high pylon sign is located adjacent to the Port Wakefield Road; it is a similar height to the approved OTR sign.
- The sign advertisement area is not extensive despite its height.
- All signage is professionally designed, compatible with the proposed development on the land and, where located on the building, are integrated with the design in form, siting and colour.
- The signage does not in my view result in clutter, it does not move flash or rotate and as such is not a distraction to drivers.

Economically, McDonald's restaurants of the size proposed provide employment opportunities for up to 100 people on a full and part time basis, but particularly for younger people within the surrounding area.

McDonald's Australia is a nationally accredited Registered Training Organisation (RTO). The organisation offers accredited courses to crew including Certificate II in Retail Operations and Certificate III in Retail Supervision; Restaurant managers, through course work, on the job skills

and experience can complete Certificate IV in Retail Management, Advanced Diploma in Business and/or an MBA qualification.

Employing local residents means that wages come back into the local economy. Similarly, the utilisation of local resources for services, maintenance and repair required by the restaurant will filter back to the community. Estimates for wages local services, maintenance & repair can be expected to exceed one million dollars annually.

Socially, McDonald's stores provide sponsorship of local initiatives and community groups: Each store is committed to contributing to the community in which it operates, and this often takes the form of local sponsorships and charitable donations. Many McDonald's Licensees are also involved in educational programs providing mentorship to local high-school students on the principles of running a business.

On a broader basis Ronald McDonald House Charities provide a 'home away from home' for the families of children with serious illness. Since 1981, RMHC has provided over 35,000 Australian families with more than 750,000 nights of accommodation. McDonald's pays all administration & management costs for RMHC, with many employees volunteering their own time. Each restaurant plays a vital role in fundraising on behalf of the charity within the local community and donations through McDonald's marketing cooperative.

The provision of employment and training initiatives by McDonald's at both the local, state and national level will have a positive economic and social impact on the locality.

6.0 CONCLUSION

Notwithstanding the proposed development is a non-complying land use; having regard to the foregoing brief assessment of the development, I am of the opinion that sufficient merit exists for the application to be assessed pursuant to Development Regulation 17(3)(b).

I note that the Council has recently assessed and approved the adjoining OTR development(s), which indicates that the Council is satisfied that commercial development in the locality is suitable notwithstanding the current zoning.

I would be happy to meet with relevant Council staff as required to discuss the application and reiterate my earlier comments that the plans at this time are simply proposal plans for the purposes of section 17(1), and further changes may be made in consultation with Council or as advised by McDonald's specialist consultants.

Should you have any queries relating to the proposed development, please do not hesitate in contacting me on 81307222 or by email dhutchison@accessplanning.com.au.
Yours sincerely,



David Hutchison BA MPIA CPP
ACCESS PLANNING (SA) PTY LTD



24th October 2020

8123SOE

Chief Executive Officer
City of Salisbury
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Attention: Ms. Karyn Brown

Dear Karyn,

**RE: PROPOSED RESTAURANT (INCLUCING ASSOCIATED DRIVE-THROUGH SERVICE,
CAR PARKING AND SIGNAGE) – 859 PORT WAKEFIELD ROAD, BOLIVAR**

1.0 INTRODUCTION

This report has been prepared in relation to an application by McDonald's Australia Limited involving the establishment of a restaurant with associated drive-thru, carparking, landscaping and signage at 859 Port Wakefield Road, Bolivar.

The land is located substantially in the Primary Production zone and partly within the Open Space zone, Policy Area 15.

As the site straddles the zone boundary, both sets of zone provisions are required to be taken into consideration in an assessment of the development.

Either way a shop is listed as being a non-complying development in both zones subject to some floor area limitations, which in this instance are exceeded for both zones.

Additionally, an advertisement is listed as being a non-complying development as the land is adjacent to a road with posted speed limit in excess of 80kph, noting that the sign area exceeds the exceptions otherwise applicable to make this aspect of the land use a consent activity.

By letter dated 19th June 2020, Council advised that it had resolved to proceed with an assessment of the application and has requested that a Statement of Effect be prepared. The following is a Statement of Effect prepared on behalf of the applicant pursuant to the requirements of Regulation 17(5) of the Regulations.



The report includes:

- a description of the nature of the development and the nature of its locality;
- a statement as to the provisions of the Development Plan which are relevant to the assessment of the proposed development;
- an assessment of the extent to which the proposed development complies with the provisions of the Development Plan; and
- an assessment of the expected social, economic and environmental effects of the development on its locality.

In preparing this report I can confirm that I am familiar with the subject land and its locality, have reviewed the proposal plans and supporting documents, had regard to the Development Act 1993 and the Regulations and assessed the proposal against the relevant provisions of the City of Salisbury Enfield Development Plan.

2.0 SUBJECT LAND AND LOCALITY

The subject land is situated at the intersection of Hodgson Road and Port Wakefield Road Bolivar. Hodgson Road is a link road between Port Wakefield Road and Northern Expressway (NEXY) and provides access to the site via the On the Run development which occupies the northern part of the land.

As indicated above, land has recently been developed with a substantial Integrated service station development, stand alone retail facilities and extensive signage. Traffic conditions in the locality have similarly been significantly modified as a consequence of works associated with the NEXY

The land is legally identified in Allotment 25 in Deposited Plan 85405 and is more particularly described in Certificate of Title Register Book Volume 6169, Folio 78; a copy of which is included with the application documents.

Port Wakefield Road is identified as a controlled access road, although the status of this is likely to change now the NEXY is diverting a large amount of traffic from Pt Wakefield Road.

There is an easement along the Port Wakefield road frontage of the land to the Natural Gas Authority of SA. The easement is shown on the proposal plans. The development does impact on the easement.

A second easement registered on the title is located some distance from the development site and is not impacted by it.

The land abuts the alignment of the NEXY to the west, Port Wakefield Road, then largely open space to the east, open space to the south and to the north the premises of the White Horse Inn Hotel.

Port Wakefield Road is identified as a primary arterial road. Hodgson Road is a secondary arterial Road. There is no meaningful current traffic data for these roads at present.

The subject land and locality is shown in more detail in the Figures 1 below.



Figure 1: Subject land and immediate locality

Source: SAAPA

3.0 THE PROPOSAL

The proposal involves the establishment of a restaurant with an integrated drive through, car parking, landscaping and signage as described below.

Building

Construction of a single storey contemporary designed restaurant/café/take away and drive through takeaway facility to be located at the southern end of the land.

The proposed building has a floor area of 400m² accommodating the dining room, cafe and serving facilities, kitchen and 'back of house' storage and staff facilities/amenities, patron amenities, service area (corral) and a small play-land area for younger children. Two serveries are located in the western elevation of the building to serve patrons of the drive through.

The building adopts a simple form comprising a 6.075m high parapet which screens the roof and roof mounted plant, with an elevated, glazed feature corner rising 5m which accommodates the play facilities in the playland area and allows extensive access to natural light to this aspect of the building.

Significant articulation is provided to the building by way the use of a variety of building material and colour. Vertical and horizontal projections and the use of shade canopies and window placement/building openings highlight various aspects of the land use, such as defining service points, building entries and windows. Signage is also incorporated as a design feature and is coordinated throughout the development.



The building exhibits a substantial setback of some 36m from Port Wakefield Road, being 24.26m inside the High-Pressure gas pipeline easement.

Access/loading bay/car parking/bicycle parking

Access to the development will be by way of existing crossovers presently servicing the OTR development which will provide entry into the one-way drive through lane and an entry and exit to and from the main car park back on to the OTR site thence to the adjoining road network.

Off-street parking is provided for 39 vehicles, including, two stacked staff car parks, one disabled persons car park and two waiting bays. The access ways and parking will be designed and constructed to meet current AS 2890.1.

A loading bay is located adjacent to the southern elevation of the proposed building. This loading bay is located and designed to allow a heavy rigid vehicle (up to 11.6 metres in length) to enter and exit the site in a forward direction.

Trading Hours

The proposed hours of operation of the facility will be 24 hours per day, seven days of the week.

Signage/Flagpoles

There will be a range of signs fixed to the building in accord with the high standard of McDonald's corporate signage. They will include:

- A freestanding pylon sign located adjacent to the Port Wakefield Road adjacent the entry to the land. This sign will be ten (12) metres in height, internally illuminated and will feature corporate signage (McDonald's "Big M" and "McCafe" and an information sign advising of the opening times of the facility);
- Ancillary signs including wall signs, directional signs, menu board signs, speed signs, and pedestrian access signs are also proposed.

Waste Management

Commercial waste management standards will apply for this facility with two commercial bins located in the Corral area. McDonald's restaurant staff will conduct litter patrols to keep the restaurant grounds and surrounds clean.

Fencing and Landscaping

The existing rural post and wire fencing is to be maintained to the Port Wakefield Road frontage and to the southern property boundary, the site will otherwise not be fenced.

Extensive landscaping is to be located around the boundary of the site boundary with landscaping beds varying from 1m to 15m wide.

A detailed landscape plan forms part of the application documents.

McDonald's approach to landscaping is for a high standard of planting (using mature stock for feature trees) and regular ongoing maintenance e. Water sensitive design is a feature of McDonald's landscaping philosophy.



There is a garden store in the corral for storage of garden tools and materials.

4.0 NATURE OF DEVELOPMENT

With reference to Schedule 1 of the Development Regulations 1993, a shop is defined as follows:

shop means—

- (a) premises used primarily for the sale by retail, rental or display of goods, foodstuffs, merchandise or materials; or
- (b) a restaurant; or
- (c) a retail showroom; or
- (d) a personal service establishment,

Whilst the proposal is for a restaurant (shop), the scheme also includes drive-thru, car parking, fencing and the provision of landscaping. However, these aspects of the development are associated and ancillary to the proposed restaurant use of the land.

5.0 DEVELOPMENT ASSESSMENT

The subject land is situated within the Primary Production and Open Space zone, the latter falling also into a policy area (15) as identified on Map Sal/31 of the Salisbury Council Development Plan, consolidated 4 April 2019.



Figure 3: Zone and policy areas affecting the subject land

I have examined the provisions of the Development Plan, both as it applies generally to the Salisbury Council and specifically to the Primary Production and Open Space zone and consider the following provisions of the Development Plan to be most relevant to an assessment of the application at hand.

5.1 Relevant provisions of the Development Plan

Primary Production
 Objectives 1 & 3
 Principles , 2, 6

Open Space
 Objectives 1, 2, 3, 4, 5, & 6
 Desired Character
 Principles 2

Procedural Matters – non complying development

Recreation Policy Area 15
 Objectives 1
 Principles 1, 2



Procedural Matters – non complying development

General Section

Advertisements
Objectives 1 to 3
Principles 1, 2, 5, 6, 9, 10, 13, 14, 15, 18, 20, 21

Crime Prevention
Objective 1
Principles 1, 2, 3, 4, 5, 6

Interface Between Land Uses
Objectives 1, 2 & 3
Principles 1, 6, 8, 12

Transportation and Access
Objective 2
Principles 2, 5, 8, 11, 13, 19, 23, 25, 27, 28, 32, 33, 34, 35, 38, 39, 40

Centres and Retail Development
Objectives 1, 2, 5
Principle 12

Design and Appearance
Objective 1 & 3
Principles 1, 2, 3, 8, 9, 11, 12, 14, 15

Landscaping, Fences and Walls
Objectives 1
Principles 1

Not all of the above provisions have been directly referenced in the following report as there is a high degree of repetition evident in the policy approaches set out in the objectives and principles under each of the headings above.

5.2 Appropriateness of the proposed use

As referenced above the subject land has a long history on non-primary production use. It has never, in my understanding been used for primary production purposes. It is adjoined to the east by a detention basin, to the south by open space and is otherwise bounded by roads. It is highly unlikely the subject site will ever be contemplated for development other than those involving commercial land uses and it certainly has no future for rural uses in accordance with the current zoning.

The intrusion of the development into the Open Space zone appears to be more an issue with the alignment of the zone boundary relative to the property boundaries rather than any conscious decision to intrude across the zone boundary into the Open Space zone.

From a policy perspective, traditionally the Development Plan establishes a hierarchy of Centres to accommodate a range of “shopping, administrative, community, education, religious and recreational facilities located in integrated centres”. The Plan goes on to identify that there are and will be community benefits in the grouping of a wide range of such facilities in integrated Centres; namely the shared use of facilities, provision of meeting places and access to public and private transport. (see General Section, Centres and Retail Development Objectives 1, 2 & 5 and Principle 12).

General Section, Centres and Retail Development, principle 12 provides some guidance regarding the location of shops outside of designated centre Zones.

12 A shop or group of shops located outside of zones that allow for retail development should:



- (a) be of a size and type that will not hinder the development, function or viability of any centre zone
- (b) not demonstrably impede the current and future commercial viability of any designated centre
- (c) be developed taking into consideration its effect on adjacent development.

In this regard, I make the following comments:

- Fast food restaurants such as McDonald's are a destination in their own right and do not form part of the day to day shopping needs of the community that necessitate a Centre Zone location;
- Fast food restaurants are traditionally sited on arterial roads, from which they draw a large percentage of their custom. On average some 60% of custom is generated by the drive-thru facility, whilst the vast majority of customers attend the stores by car rather than public transport.
- The area required accommodating the drive-thru lanes and service access limits their ability to be sited in local centre zones and limited access to arterial roads from larger centres militates against their location in larger, established centres.
- The hours of operation of the proposed store differ significantly from normal retail outlets (24 hours) with peak evening trade occurring at times when other outlets are often shut, such that any benefits to other shops in a Centre zone deriving from the customers to the restaurant are minimal;
- Given that the customer base of, and reason to visit a fast food restaurant differs from the day to day shopping trip normally associated with the reason to visit a shopping centre, it is not considered that its location outside of a designated Centre will detract from the function or viability; or result in the deterioration of nearby centre zones;
- Most Centre Zones in the area share a close common boundary with adjoining residential development and to this end have many of the same attributes as the subject land in its relationship to adjoining sensitive land uses.
- The development has been designed having regard to its relationship with the adjoining commercial development and its relationship to the road network. It shares common access, occupies its own discreet site and is located In this regard the building is set as far away as possible from the boundary with neighbouring commercial uses, and land landscaping and acoustic treatments will not only minimise impacts from the proposed use but also the noise generated by traffic movement on the adjoining arterial road network.
- Having regard to the attributes of the land as referred in part 2.0 above, I am satisfied that the property is an appropriate location for the proposed restaurant.

For the above reasons, I consider the proposed fast food restaurant to be an appropriate form of development on the subject land, and one which would not be likely to compromise the function or role of an existing centre or have an unacceptable impact on adjoining development.

5.3 Primary Production Zone and Open Space Zone

Primary Production Zone

Objectives

- 1 The long term continuation of primary production.
- 3 Allotments of a size and configuration that promote the efficient use of land for primary production.

Principles of Development Control



- 1 The following forms of development are envisaged in the zone:
 - bulk handling and storage facility
 - commercial forestry
 - dairy farming
 - farming
 - horticulture
 - tourist accommodation (including through the diversification of existing farming activities and conversion of farm buildings).
- 2 Development listed as non-complying is generally inappropriate.
- 6 A shop should be:
 - (a) ancillary to primary production or processing uses or tourist accommodation or other tourist development
 - (b) located on the same site as the primary use.

**Open Space Zone
Objectives**

- 1 A zone:
 - (a) in which the open space character is preserved to provide a visual contrast to the surrounding urban area
 - (b) comprising open space that accommodates a range of public and private activities in an open and natural setting, including:
 - (i) passive and active recreation land uses
 - (ii) habitat conservation and restoration
 - (iii) associated administrative, visitor and spectator facilities.
- 2 Land within the Metropolitan Open Space System (MOSS) contributing to regional open space networks and providing an open, natural and rural character accommodating low-scale uses such as nonintensive agriculture
- 3 Public land within the MOSS that provides for recreation areas and facilities, sporting facilities and conservation of the open, natural character.
- 4 A linear park that:
 - (a) provides an open space corridor across Greater Adelaide/Regional South Australia
 - (b) promotes the use, awareness and preservation of the environment and enhancement of natural or semi natural habitats for the movement of wildlife and conservation of biodiversity.
- 5 Development that contributes to the desired character of the policy area.

Desired Character

The zone provides for a range of passive and active recreation land uses, and associated spectator facilities and administrative functions.

Principle

- 2 Development listed as non-complying is generally inappropriate.

Recreation Policy Area 15

Objectives

- 1 A policy area accommodating sporting, entertainment, cultural and recreational activities and associated spectator and administrative facilities.

Principle

- 1 The following forms of development are envisaged in the policy area.



- shops or groups of shops ancillary to recreation development
- 2 A shop or group of shops should only be developed where:
- (a) it is ancillary to recreation and sport development
 - (b) the total gross leasable area is less than 80 square metres.

To the extent that is relevant the zone and policy area provisions do not limit development in the zone to only primary production land uses.

That said, it clear that the land has not been historically used for Primary Production purposes for many years and the present development of the bulk of the land as an integrated petrol filling station development clearly prohibits the remainder of the property being able to be used for any kind of rural production purposes.

The incorporation of part of the land into the Open Space zone is unusual, as in normal circumstances zone boundaries follow cadastre. I do note however that the boundary is defined by dimensions which would appear to indicate that was a deliberate act. In the absence of any evidence to the contrary it is unusual to include private land in an open space zone but I note that for the most part, the development proposed that falls within the Open Space zone is low in scale, small in mass and mostly comprises driveway and landscaping. Open fencing will maintain an outlook into the Open Space zone such the impact of the development on this zone will be minimal.

For all the above reasons I have concluded that the land is not fundamentally suited for use for primary production or open space purposes. The size of the land holding, its location and current use questions the appropriateness and viability of converting, maintaining and improving Primary Production activities on the site. I am supported in this position by the Council's recent approval of a petrol filling station and restaurants on the land.

6.3 Advertisements

Advertisements of the type proposed are non-complying in the zone.

General Section Objectives

- 1 Urban and rural landscapes that are not disfigured by advertisements and/or advertising hoardings.
- 2 Advertisements and/or advertising hoardings that do not create a hazard.
- 3 Advertisements and/or advertising hoardings designed to enhance the appearance of the building and locality.

Principles

- 1 The location, siting, design, materials, size, and shape of advertisements and/or advertising hoardings should be:
 - (a) consistent with the predominant character of the urban or rural landscape
 - (b) in harmony with any buildings or sites of historic significance or heritage value in the area
 - (c) coordinated with and complement the architectural form and design of the building they are to be located on.
- 2 The number of advertisements and/or advertising hoardings associated with a development should be minimised to avoid:
 - (a) clutter
 - (b) disorder
 - (c) untidiness of buildings and their surrounds.



- 5 Advertisements and/or advertising hoardings should:
 - (a) be completely contained within the boundaries of the subject allotment
 - (b) be sited to avoid damage to, or pruning or lopping of on-site landscaping or street trees
 - (c) not obscure views to vistas or objects of high amenity value.
- 6 Advertisements and/or advertising hoardings should not be erected on:
 - (d) residential land.
- 9 Advertisements should be designed to conceal their supporting advertising hoarding from view.
- 10 Advertisements should convey the owner/occupier and/or generic type of business, merchandise or services using simple, clear and concise language, symbols, print style and layout and a small number of colours.
- 13 Advertisements and/or advertising hoardings should not create a hazard by:
 - (a) being so highly illuminated as to cause discomfort to an approaching driver, or to create difficulty in the driver's perception of the road or persons or objects on the road
 - (b) being liable to interpretation by drivers as an official traffic sign, or convey to drivers information that might be confused with instructions given by traffic signals or other control devices, or impair the conspicuous nature of traffic signs or signals
 - (c) distracting drivers from the primary driving task at a location where the demands on driver concentration are high
 - (d) obscuring a driver's view of other road or rail vehicles at/or approaching level crossings, or of pedestrians or of features of the road that are potentially hazardous (eg junctions, bends, changes in width, traffic control devices).
- 14 Any internally illuminated advertising signs and/or advertising hoardings which utilise LED, LCD or other similar technologies should be located a minimum of 80 metres from traffic signals, level crossings and other important traffic control devices.
- 15 Freestanding advertisements and/or advertising hoardings should be:
 - (a) limited to only one primary advertisement per site or complex (except where a site has multiple road frontages, in which case, one freestanding advertising hoarding per road frontage is appropriate)
 - (b) of a scale and size in keeping with the desired character of the locality and compatible with the development on the site.
- 18 Freestanding advertisements should not exceed the total height (measured from natural ground level) as specified within the following table:

Location of freestanding advertisement	Total height (in metres)
In all other locations	3

- 20 Advertisements and/or advertising hoardings incorporating any flags, bunting, streamers, or suspended objects should:
 - (a) be placed or arranged to complement and accord with the scale of the associated development
 - (b) other than flags, not be positioned higher than the building they are attached or related to
- 21 Advertising and/or advertising hoardings should not be placed along arterial roads that have a speed limit of 80 km/h or more.

The advertising is designed, coloured and sited to be integrated with the development of the site as a McDonald's restaurant, the development of which will result in a net improvement to the appearance and amenity of the site, including co-ordinated advertising, significant areas of landscaping and a high quality built form.

Advertising on the site generally is restrained with subtle advertising limited predominantly to the building where it is fully integrated with the building design and form.



The proposed 12-metre-high freestanding sign is consistent with McDonald's sites nationally.

In regard to the subject land, it has an extensive frontage to Port Wakefield Road measuring in excess of 246m for which only 1 freestanding sign is exists. Placement of one additional freestanding signage with identical height to the existing sign for the OTR Development is such that sufficient space is provided between the signs to avoid what might be described as signage clutter.

The proposed approach to advertising is for signage that is well designed and professionally constructed, with a limited range of colours and a restrained message. The signs will not be a discordant element in a locality characterised by extensive arterial road frontages, high traffic volumes and a somewhat discordant array overhead high and low voltage power lines and supporting poles.

Ancillary signage is typical of a fast food restaurant of this nature; they are located well within the site, are of low scale and are not generally or highly visible to passing traffic on the surrounding road network as they are viewed for the most part against the background of the building. As such they will have minimal impact on the amenity of the locality.

The signs are not of an excessive scale nor are they of a nature that will distract motorists from the primary task of driving, and in fact they are a common form of signage associated with fast food restaurants on busy arterial roads, often sited at or close to controlled road intersections.

The advertising is consistent with the hierarchy of signage sought by the Development Plan with the directional and information signs forming a high proportion of the lower scale signs on the site.

The proposed signage does not move, rotate (other than in respect to the menu board which is changed once a day from the breakfast menu to the afternoon/evening menu) or incorporate flashing lights, and lighting lux is not so high as to impact on the safety of passing traffic.

No portable advertising is proposed.

While the height of the freestanding sign and the flag poles do not meet the requirements of the zone, the signage will not detract unreasonably from the amenity of the locality and warrants support given the specific attributes of the land referred to above.

6.4 Crime Prevention

General Section Objective

- 1 A safe, secure, crime resistant environment where land uses are integrated and designed to facilitate community surveillance.**

Principles

- 1 Development should be designed to maximise surveillance of public spaces through the incorporation of clear lines of sight, appropriate lighting and the use of visible permeable barriers wherever practicable.**
- 2 Buildings should be designed to overlook public and communal streets and public open space to allow casual surveillance.**
- 3 Development should provide a robust environment that is resistant to vandalism and graffiti.**
- 4 Development should provide lighting in frequently used open spaces pedestrian areas and other vulnerable parts of centres and residential areas including those:**
 - (a) along dedicated cyclist and pedestrian pathways, laneways and access routes**
 - (b) around public facilities such as toilets, telephones, bus stops, seating, litter bins, automatic teller machines, taxi ranks and car parks.**



- 5 Development, including car park facilities should incorporate signage and lighting that indicate the entrances and pathways to, from and within sites.
- 6 Landscaping should be used to assist in discouraging crime by:
 - (a) screen planting areas susceptible to vandalism
 - (b) planting trees or ground covers, rather than shrubs, alongside footpaths
 - (c) planting vegetation other than ground covers a minimum distance of two metres from footpaths to reduce concealment opportunities.

McDonald's works closely with the police to ensure that site security is maintained at the highest level and the site design incorporates a range techniques to achieve this goal, including:

- lighting throughout the site including within the car parking area, entrance and generally around the building;
- clear staff/public surveillance of the car park area from within the building;
- video surveillance to maximise safety for McDonald's employees and customers;
- landscaping with groundcovers and feature trees to maintain appropriate lines of sight, thus maximising surveillance into the site from the surrounding road network;
- Staff training to cope with emergency situations.

It is in McDonald's best interest to maintain a safe environment at all times for both customers and employees of the business.

Similarly building finishes are robust with a combination of site security measures referred to above and 24 hour operation which minimises the potential for anti-social behavior and graffiti vandalism.

6.5 Design and Appearance

General Section

Objectives

- 1 Development of a high architectural standard that responds to and reinforces positive aspects of the local environment and built form.
- 3 The amenity of localities not impaired by the appearance of land, buildings and objects.

Principles of Development Control

- 1 The design of a building may be of a contemporary nature and exhibit an innovative style provided the overall form is sympathetic to the scale of development in the locality and with the context of its setting with regard to shape, size, materials and colour.
- 2 Buildings should be designed and sited to avoid creating extensive areas of uninterrupted walling facing areas exposed to public view.
- 3 Buildings should be designed to reduce their visual bulk and provide visual interest through design elements such as:
 - (a) articulation
 - (b) colour and detailing
 - (c) small vertical and horizontal components
 - (d) design and placing of windows
 - (e) variations to facades.
- 8 Structures located on the roofs of buildings to house plant and equipment should form an integral part of the building design in relation to external finishes, shaping and colours.
- 9 Building design should emphasise pedestrian entry points to provide perceptible and direct access from public street frontages and vehicle parking areas.



- 11 Buildings, landscaping, paving and signage should have a coordinated appearance that maintains and enhances the visual attractiveness of the locality.**
- 12 Buildings (other than ancillary buildings or group dwellings) should be designed so that their main facade faces the primary street frontage of the land on which they are situated.**
- 14 Development should be designed and sited so that outdoor storage, loading and service areas are screened from public view by an appropriate combination of built form, solid fencing and/or landscaping.**
- 15 Outdoor lighting should not result in light spillage on adjacent land.**

The above provisions promote the enhancement of visual amenity throughout the Zone through appropriate siting, high quality design and landscaping.

Whilst commercial in nature, the scale, mass and built form of the proposed development will not detract from the character or amenity of the locality. The proposal is viewed as an appropriate form of development within the zone given the site's constraints and existing development within the locality. Proposed landscaping will soften the built form and enhance the proposed development.

The proposed building is unashamedly commercial in appearance and this derives as much from the building form and finishes as from the car parking and range of signage proposed.

The building is in an area that has a dominant commercial character deriving from the range of uses aligned along Port Wakefield Road where buildings are generally commercial/industrial in nature, scale, appearance, and form.

Views along Port Wakefield Road are dominated by a mixture of traffic, power poles and overhead wires and lights, with overhead infrastructure being a feature in the immediate locality along the western side of Port Wakefield Road.

In contrast to some of the commercial development in the locality, particularly the nearby Krispy Kreme shows that the overall height, bulk, and scale of the proposed restaurant building will be sympathetic to its interface location with adjoining development.

Building design is of high quality and low scale, with co-ordinated colour schemes, signage, setbacks, and landscaping that present a neat and tidy appearance.

The use of awning, openings, and windows, blade walls and varying external materials help to divide the building and generate visual interest. At distance, this material appears uniform in its matte colours; however, when close provides visual detailing to the building through different material finishes.

Site coverage is low and provides ample opportunity for setback from the boundaries, site landscaping and car parking to be accommodated easily on the site.

All areas not required for vehicle parking and maneuvering will be extensively landscaped, with trees and shrubs to provide shade and additional site screening elements.

Building setbacks from Port Wakefield Road are extensive and well more than the relevant Development Plan provisions and car parking is readily visible and access to passing traffic on these roads.

No outdoor storage areas are required, all rubbish and other storage requirements being accommodated within dedicated areas in the building.



Service access is catered for and is undertaken out of peak hour trade periods to avoid conflict between service vehicles, customer vehicles and pedestrians, with ample maneuvering area to enable service vehicles to enter and exit the site in a forward direction.

Amongst other things the development;

- is well-designed and of a character and form that will enhance the attractiveness of this part of Port Wakefield Road, with attention to built form, design, siting and landscaping all intended to improve the amenity of the locality;
- addresses interface issues, such as noise, visual impact, and privacy that arise due to the proximity of housing;
- will enhance the character of the area through high quality building design and extensive site landscaping;
- provides co-ordinated signage commensurate with the use of and dimensions of the land;
- adopts careful design to minimise impact on residential amenity;
- buffers adjacent housing by screen plantings and an acoustic fence.

6.6 Interface between Land Uses

General Section

Objectives

- 1 Development located and designed to prevent adverse impact and conflict between land uses.
- 2 Protect community health and amenity from adverse impacts of development.
- 3 Protect desired land uses from the encroachment of incompatible development.

Principles

- 1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:
 - (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants
 - (b) noise
 - (c) vibration
 - (d) electrical interference
 - (e) light spill
 - (f) glare
 - (g) hours of operation
 - (h) traffic impacts.
- 6 Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.
- 8 Development with the potential to emit harmful or nuisance-generating air pollution should incorporate air pollution control measures to prevent harm to human health or unreasonable interference with the amenity of sensitive uses within the locality.
- 12 Chimneys or exhaust flues associated with commercial development (including cafes, restaurants and fast food outlets) should be designed to ensure they do not cause a nuisance or health concerns to nearby sensitive receivers by:
 - (a) incorporating appropriate treatment technology before exhaust emission are released to the atmosphere
 - (b) ensuring that the location and design of chimneys or exhaust flues maximises dispersion and takes into account the location of nearby sensitive uses.

The manner in which the site development addresses interface issues associated with noise has already been dealt with above as have matters pertaining to 24 hour opening, and site traffic.

To the extent that is relevant the site is bounded the NEXY and Port Wakefield Road that are greater noise producers than the proposed development. This is particularly evident in use of those



roads by articulated vehicles and B-Doubles when they are shifting through gears At 150 metres to the nearest sensitive receptors the proposed development is adequately separated by the above.

The site will be lit for security purposes, but lights will be directed and shielded in such a manner to avoid light spill into the adjoining residential properties.

The key sources of potential odours from the development are associated with cooking and waste storage on the premises.

With regard to cooking, the kitchen will be operated in accordance with the Food Standards Code under the Food Act 2003 and Australian Standard 4674 – Design, Construction and Fit out of Food Premises; and will accord with the Environment Protection (Air Quality) Policy 1994, under the Environment Protection Act, both of which are intended to minimise the potential for the site to generate cooking odours.

McDonald's adopt a strict regime of cleaning and maintenance of exhaust systems to ensure that any build-up of fat is removed from filters and exhaust hoods daily and from ducting and mechanical plant monthly.

All waste will be stored in the proposed waste storage area (corral). This area is of a sufficient size to accommodate the number of required bins and is cleaned regularly as part of the premises on-going operation.

The design and operation of the restaurant will incorporate an exhaust ventilation system which complies with the relevant Australian Standard for cooking odour extraction, and operations will involve minimising any odour that may be discharged by the development.

6.8 Landscaping Fences and Walls

General Section

Objectives

- 1 The amenity of land and development enhanced with appropriate planting and other landscaping works, using locally indigenous plant species where possible.

Principles of Development Control

- 1 Development should incorporate open space and landscaping in order to:
 - (a) complement built form and reduce the visual impact of larger buildings (eg taller and broader plantings against taller and bulkier building components)
 - (b) enhance the appearance of road frontages
 - (c) screen service yards, loading areas and outdoor storage areas
 - (d) minimise maintenance and watering requirements
 - (e) enhance and define outdoor spaces, including car parking areas
 - (f) provide shade and shelter
 - (g) assist in climate control within buildings
 - (h) maintain privacy
 - (i) maximise stormwater re-use
 - (j) complement existing native vegetation
 - (k) contribute to the viability of ecosystems and species
 - (l) promote water and biodiversity conservation.

Landscaping comprises an area of some 20% of the site.

A landscaping plan is has been prepared and includes the following features which have been devised to enhance the development by:

- establishing landscaping beds of between 0.9 and 5.5 metres in width adjacent to all site boundaries;



- utilizes plants that will grow to screen the fences located along the northern boundary of the land;
- locating the larger proportion of the overall landscaping adjacent the street boundary to maintain a quality appearance to the public realm;
- using plant species that will include grasses, medium height shrubs and small trees in scale with the proposed building but designed not to impede casual surveillance;
- using low-height planting adjacent the access driveway to maintain adequate sight lines and screen headlight glare to on-coming traffic on both Briens and Grand Junction Roads;
- Adopting a planting schedule that includes native plants, to foster the principles of Water Sensitive Urban Design.

McDonald's prides itself on the ongoing maintenance of landscaping to a high standard which is important to the overall public presentation of the site and which will improve the appearance of the site and in turn improve the amenity of the locality.

6.9 Transportation and Access

General section Objectives

- 2 Development that:
 - (a) provides safe and efficient movement for all motorised and non-motorised transport modes
 - (b) ensures access for vehicles including emergency services, public infrastructure maintenance and commercial vehicles
 - (c) provides off street parking
 - (d) is appropriately located so that it supports and makes best use of existing transport facilities and networks.

Principles of Development

- 2 Development should be integrated with existing transport networks, particularly major rail and road corridors as shown on Location Map and Overlay Maps - Transport, and designed to minimise its potential impact on the functional performance of the transport networks.
- 5 Land uses that generate large numbers of visitors such as shopping centres and areas, places of employment, schools, hospitals and medium to high density residential uses be located so that they can be serviced by existing transport networks and encourage walking and cycling.
- 8 Development should provide safe and convenient access for all anticipated modes of transport.
- 11 Development should discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive land uses.
- 13 Development should provide for the on-site loading, unloading and turning of all traffic likely to be generated.
- 19 Developments should encourage and facilitate cycling as a mode of transport by incorporating end-of-journey facilities including:
 - (a) showers, changing facilities, and secure lockers
 - (b) signage indicating the location of bicycle facilities
 - (c) secure bicycle parking facilities provided at the rate set out in Table Sal/3 - Off Street Bicycle Parking Requirements.
- 23 Development should be provided with safe and convenient access which:
 - (a) avoids unreasonable interference with the flow of traffic on adjoining roads
 - (b) provides appropriate separation distances from existing roads or level crossings
 - (c) accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision
 - (d) is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.



- 25 The number of vehicle access points onto arterial roads as shown on Overlay Maps - Transport should be minimised, and where possible access points should be:
 - (a) limited to local roads
 - (b) shared between development
- 27 Development with access from roads with existing or projected traffic volumes exceeding 6000 vehicles per day should be sited to avoid the need for vehicles to reverse onto or from the road.
- 28 Development with access from arterial roads or roads as shown on Overlay Maps - Transport should be sited to avoid the need for vehicles to reverse on to the road.
- 34 Development serviced by heavy vehicles should:
 - (a) be located where the access roads have a carriageway of at least 9.0 metres
 - (b) provide a turning circle of at least 30 metres in diameter to ensure the safe and convenient manoeuvring of vehicles on site.
- 32 Development should provide off-street vehicle parking and specifically marked disabled car parking places to meet anticipated demand in accordance with Table Sal/2 - Off Street Vehicle Parking Requirements or Table Sal/2A - Off Street Vehicle Parking Requirements for Designated Areas (whichever applies) unless an agreement is reached between the Council and the applicant for a reduced number of parking spaces where one of the following applies:
 - (b) it can be demonstrated that fewer car parks would be required to meet the car parking needs associated with the development.
- 33 Development should be consistent with Australian Standard AS 2890 Parking facilities.
- 34 Vehicle parking areas should be sited and designed to:
 - (a) facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development
 - (b) include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network
 - (c) not inhibit safe and convenient traffic circulation
 - (d) result in minimal conflict between customer and service vehicles
 - (e) avoid the necessity to use public roads when moving from one part of a parking area to another
 - (f) minimise the number of vehicle access points to public roads
 - (g) avoid the necessity for backing onto public roads
 - (h) where practical, provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for access points
 - (i) not dominate the character and appearance of a centre when viewed from public roads and spaces
 - (j) provide landscaping that will shade and enhance the appearance of the vehicle parking areas.
- 35 Vehicle parking areas should be designed to reduce opportunities for crime by:
 - (a) maximising the potential for passive surveillance by ensuring they can be overlooked from nearby buildings and roads
 - (b) incorporating walls and landscaping that do not obscure vehicles or provide potential hiding places
 - (c) being appropriately lit
 - (d) having clearly visible walkways.
- 38 Vehicle parking areas should be sealed or paved in order to minimise dust and mud nuisance.
- 39 To assist with stormwater detention and reduce heat loads in summer, outdoor vehicle parking areas should include landscaping.



40 Vehicle parking areas should be line-marked to delineate parking bays, movement aisles and direction of traffic flow.

In general terms the car park layout is connected to an existing access from Port Wakefield Road. The access arrangements for this facility are capable of handling the traffic flows associated with the drive-thru facility. In part the design and positioning of the existing access arrangements have dictated the design and layout of the development.

This layout for the most part located the parking areas to the east and northern side of the building. The corner location is such that the car park will be visible from both Port Wakefield Road and the integrated service station complex, and thus to passing traffic most likely to access the site. The visibility of the car park to passing traffic makes it more readily identifiable and the access points more obvious than if the car parks were completely located at the rear of the building.

The layout of the site provides outlook into the site from the adjoining roads across a landscaped area which enhances the security of the site by having this public area open to observation from the adjoining roads, as well as from within the building and neighbouring land uses.

Service delivery occurs once daily with a combination of goods delivery and rubbish collection.

Delivery times are arranged to avoid peak trading periods and the site has been designed to provide access for service vehicles up to 11.6 metres in length.

The proposed restaurant will generate in the order of 921 vehicle movements per day. The amount of seating is estimated to be approximately 100 in total. Therefore, the 39 car parking spaces proposed exceeds the required 30 parking spaces prescribed in Table Sa1/2 relating to off street parking requirements for a restaurant.

Car parking and access otherwise accords with the relevant Australian Standards, car parking provision is sufficient to meet peak demand and in complies with the parking requirements contained in Tables Sa1/2.

7.0 ENVIRONMENTAL, ECONOMIC AND SOCIAL EFFECTS

The proposed restaurant development is expected to have positive environmental, economic and social impacts on its locality.

From an environmental perspective, the landscaping will include principally low-maintenance native plants.

Economically, McDonald's restaurants of the size proposed provide employment opportunities for up to 100 people on a full and part time basis but particularly for younger people within the surrounding area.

Such stores have a rotating roster of up to 100 staff, including 20 full time positions consisting of managers, crew, maintenance & marketing staff.

McDonald's Australia is a nationally accredited Registered Training Organisation (RTO). The organisation offers accredited courses to crew including Certificate II in Retail Operations and Certificate III in Retail Supervision. Restaurant managers, through course work, on the job skills and experience can complete Certificate IV in Retail Management, Advanced Diploma in Business and/or an MBA qualification.

Employing local residents means that wages come back into the local economy. Similarly, the utilisation of local resources for services, maintenance and repair required by the restaurant will filter back to the community. Estimates for wages local services, maintenance & repair can be expected to exceed one million dollars annually.



Socially, McDonald's stores provide sponsorship of local initiatives and community groups: Each store is committed to contributing to the community in which it operates, and this often takes the form of local sponsorships and charitable donations. Many McDonald's Licensees are also involved in educational programs providing mentorship to local high-school students on the principles of running a business.

On a broader basis Ronald McDonald House Charities provide a 'home away from home' for the families of children with serious illness. Since 1981, RMHC has provided over 35,000 Australian families with more than 750,000 nights of accommodation. McDonald's pays all administration & management costs for RMHC, with many employees volunteering their own time. Each restaurant plays a vital role in fundraising on behalf of the charity within the local community and donations through McDonald's marketing cooperative.

The provision of employment and training initiatives by McDonald's at both the local, state and national level will have a positive economic and social impact on the locality.

8.0 SUMMARY

A restaurant is listed as being a non-complying form of development. In addition, the 12m high pylon sign is non-complying in its own right.

Notwithstanding the above, the proposal is considered to represent an appropriate form of development for the following reasons:

- It is highly unlikely the subject site will ever be contemplated for development other than those involving non-primary production land uses. Despite the land being zoned Primary Production and minor portion Open Space; its potential for development for this use is considerably limited as the land is subject to a variety of external impacts which create a less than ideal amenity for residential use. In this regard the land is subject to high levels of noise and air pollution emissions produced by high traffic volumes on the adjoining arterial road network; which includes large numbers of heavy vehicles, often under various forms of acceleration and braking as they travel along Port Wakefield Road, or are turning onto or from Hodgson Road connection to NEXY;
- The development has been designed and sited to minimise impact on neighbouring residential development to the south-west. To this end the building is sited as far away as practicable from the adjoining houses, the ordering points and drive-thru are similarly sited away from the houses, with the drive-thru pay and pick up points adjacent to rear of the site;
- Noise attenuation and light overspill measures will be adopted to minimise impacts from these aspects of the development;
- Although the Development Plan seeks to aggregate shopping facilities in Centre Zones, Convenience restaurants such as McDonald's are a destination in their own right and do not form part of the day to day shopping needs of the community that necessitate a Centre Zone location;
- Convenience restaurants are traditionally sited on arterial roads, from which they draw a large percentage (60%) of their custom, the area required to accommodate the drive-thru lanes and



service access limits their ability to be sited in smaller centre zones, and limited access to arterial roads from larger centres militates against their location in larger, established centres;

- In Centre zones, Convenience restaurants are most often located on stand-alone sites and are not integrated with the principal shopping outlets, reflecting their status as stand-alone developments;
- The hours of operation of the proposed store differ significantly from normal retail outlets (i.e. 24 hours) with peak evening trade occurring at times when other retail outlets are often shut; such that any benefits deriving from the customers of the Convenience restaurant also shopping at other retail outlets in a Centre are minimal;
- Given that the customer base (primarily drive-thru and take away orders) of, and reason to visit a Convenience restaurant differs from the day to day shopping trip normally associated with the reason to visit a shopping centre, it is not considered that a location outside of a designated Centre will detract from the function or result in the deterioration of nearby centre zones through the erosion of the customer base;
- The subject land is isolated from nearby residential land. The design and siting of the development is intended to minimise the impact of the restaurant on the neighbouring houses and will incorporate noise attenuation measures as necessary to maintain an appropriate level of residential amenity for these properties. It is considered that any noise attenuation measures adopted to minimise the impacts arising from the proposed development will have additional significant benefits in ameliorating noise impacts already occurring in the locality and arising from the high levels of traffic on the adjoining arterial roads;
- The overall height, bulk and scale of the proposed restaurant building will be sympathetic to existing commercial buildings in the locality and the long-established commercial character along Port Wakefield Road;
- The proposal will be constructed from quality materials and together with proposed landscaping will significantly enhance the appearance and visual amenity of the subject land and locality;
- Access to the site will be taken via appropriately designed crossovers and driveways from both Port Wakefield Road and Hodgson Road;
- It is expected that the site will have the following traffic and car parking:
 - The proposed restaurant will generate in the order of 921 vehicle movements per day;
 - The development will provide 89 seats. Therefore, the 35 car parking spaces proposed exceeds the required 30 parking spaces prescribed in Table Table Sa/2 relating to off street parking requirements for a restaurant;
 - The car parking and access layout has been designed in accordance with the relevant Australian Standards;
 - The proposed loading/delivery area has been designed to allow a semi-trailer to enter and exit the site in a forward direction;
- The 12m high pylon sign is located adjacent to the existing OTR Sign on Port Wakefield Road. As such the sign is sited approximately 150 metres from the nearest house.



- The sign advertisement area is not extensive despite its height.
- All signage is professionally designed, compatible with the proposed development on the land and, where located on the building, are integrated with the design in form, siting and colour.

9.0 CONCLUSION

Notwithstanding the proposed development is a non-complying land use; having regard to the foregoing assessment of the development, I am of the opinion that sufficient merit exists for the application to be approved.

I note that the Council has recently assessed and approved the adjoining OTR development(s), which indicates that the Council is satisfied that commercial development in the locality is suitable notwithstanding the current zoning.

Accordingly, I am of the opinion that the proposal is worthy of the support of Council.

Should you have any questions regarding the above report, please do not hesitate to contact on 81307222 or at dhutchison@accessplanning.com.au.

Yours sincerely,

A handwritten signature in blue ink that reads 'D Hutchison'.

David Hutchison BA CPP PIA
ACCESS PLANNING (SA) PTY LTD

Karyn Brown

From: David Hutchison <hutch@accessplanning.com.au>
Sent: Monday, 26 October 2020 1:22 PM
To: Karyn Brown
Subject: 361/778/2020/NB - 24 Hour Fast Food Restaurant - 859-885 Port Wakefield Road, Bolivar - Response to Further Information Request, Arborist Response and Declaration of Applicant (Building Safely Near Powerlines) Form - 26 October 2020
Attachments: 361_778_2020_NB FINFO_190620.pdf; Re: Bolivar McDonald's; Electricity Declaration.pdf

Further to your request for more information re the above I advise most of the matters set out in that request have been addressed in the SoE.

McDonalds have not engaged an acoustic engineer given the substantial separation distance to the closest sensitive land use (Dwellings), the closest of which is some 150m distant from the property boundary (180m or thereabouts from the building) and separated by the development both visually and acoustically by a boundary fence. Noting that the road reserve of Port Wakefield Road is a significant source of constant 24/7 background noise that would be higher at the sensitive receiver than any noise generated by the proposed development.

For the same reason neither an odour assessment nor a traffic assessment has not been undertaken.

As you would be aware most McDonalds stores are much closer to adjoining residential development than the proposed store, and in fact most have residential land uses as near neighbours.

The issues of noise and odour impacts were addressed by the ERD Court in the REICHELDT & ORS v CITY OF CHARLES STURT & ANOR [2016] SAERDC 38 [17 November 2016]. A link to the Court decision is provided below.

https://www.austlii.edu.au/cgi-bin/viewdoc.au/cases/au/SAERDC/2016/38.html?context=1;query=McDonald%27s;mask_path=au/cases/au/SAERDC

In relation to traffic issues I can confirm all access will be by existing access points to the OTR development and that access has been designed to accommodate delivery vehicles as per the swept paths provided. The standard loading bay is designed for a 15.3m long semi trailer. Service access to the site will be via an 11.6m rigid vehicle as per the plans submitted.

The proposed layout for loading may occasionally require heavy and light vehicles to mix, however there is sufficient space for the delivery truck to manoeuvre within the proposed aisle without impacting car parking spaces. A waste collection vehicle will also utilise the same loading dock and entry and exit movements, and will be a smaller vehicle, typically a 10m long single unit truck.

On the basis of typical McDonald's loading and delivery procedures there will be a limited number of deliveries in a 24 hour period. It should be noted that the safe operation of other McDonald's stores through Australia whereby similar layouts exist and operate with little impact on adjacent car park areas and customer vehicle movements.

The only tree required to be removed is a Pepper Tree which is exotic to Australia. See email attached from the arborist.

I trust the plans and information provided to Council together with the above address the information sought by Council.

Regards

David



ACCESS PLANNING (SA) PTY LTD
235 Henley Beach Road
TORRENSVILLE SA 5031
Phone : **(08) 81307222**
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Facsimile : **(08) 81307299**
Email : hutch@accessplanning.com.au

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Attachment 2

Notice of Category 3 Development

**DEVELOPMENT ACT 1993
CITY OF SALISBURY**

NOTICE OF APPLICATION FOR CATEGORY 3 DEVELOPMENT
Pursuant to Section 38(5) of the *Development Act 1993*

An application for development has been lodged with the Council for assessment. The details are as follows:

APPLICATION NO:	361/778/2020/NB
APPLICANT:	McDonalds Australia Ltd 2 Smith St COLLINGWOOD VIC 3066
NATURE OF DEVELOPMENT:	24 HOUR FAST FOOD RESTAURANT WITH ASSOCIATED DRIVE THROUGH, SIGNAGE (INCLUDING ONE (1) TWELVE (12) METRE HIGH FREESTANDING ILLUMINATED PYLON SIGN), CARPARKING, SERVICE AREAS AND LANDSCAPING (NON-COMPLYING)
LOCATION:	859-885 Port Wakefield Road, Bolivar SA 5110
CERTIFICATE OF TITLE:	CT-6169/78
ZONE:	Primary Production, Open Space

The application may be examined at the Salisbury Community Hub located at 34 Church Street, Salisbury during business hours (8.30am – 5pm Monday to Friday) and on Council's web site at www.salisbury.sa.gov.au. Any person or body may make representations in writing, or by email to development@salisbury.sa.gov.au, concerning this application and should address their representation to the Chief Executive Officer at PO Box 8, Salisbury or representations@salisbury.sa.gov.au. Representations must be received **no later than Thursday 26th November 2020**.

Each person making a submission should indicate whether they wish to appear personally, or be represented by another party, in support of their submission. Please note that should you nominate to be heard in support of your representation, you will be required to attend a Council Assessment Panel meeting held at the Council offices, scheduled on the fourth Tuesday of each month at 6.00pm (unless otherwise advised).

Please note that, pursuant to Section 38(8) of the *Development Act 1993*, a copy of each representation received will be forwarded to the applicant to allow them to respond to all representations received.

Signed: Karyn Brown, Development Officer

Date: 13 November 2020

THIS IS THE FIRST AND ONLY PUBLICATION OF THIS NOTICE

Attachment 3

Schedule 8 Referral Response

In reply please quote: 2020/00287, Process ID: 646892
 Enquiries to: Daniel Sladic
 Telephone: 7109 7872
 E-mail: dpti.luc@sa.gov.au



Government of South Australia
 Department for Infrastructure
 and Transport

TRANSPORT PLANNING AND
 PROGRAM DEVELOPMENT

Transport Assessment

GPO Box 1533
 ADELAIDE SA 5001

ABN 92 366 288 135

23 December 2020

Ms Karyn Brown
 City of Salisbury
 PO Box 8
 SALISBURY SA 5108

Dear Ms Brown

SCHEDULE 8 - REFERRAL RESPONSE

Development No.	361/778/20
Applicant	McDonalds Australia
Location	859-885 Port Wakefield Road, Bolivar
Proposal	24 Hour fast food restaurant

I refer to the above development application forwarded to the Commissioner of Highways (CoH) in accordance with Section 37 of the *Development Act 1993*. The proposed development involves development adjacent a main road as described above.

The following response is provided in accordance with Section 37(4)(b) of the *Development Act 1993* and Schedule 8 of the *Development Regulations 2008*.

CONSIDERATION

The subject site is presently vacant and located south of the existing Service Centre located on the corner of Port Wakefield Road and Hodgson Road. At this location, Port Wakefield Road carries approximately 25,800 vehicles per day (12.5% commercial vehicles) and has a posted speed limit of 90 km/h.

This development proposes to utilise the existing access points which currently provide access to the Service Centre. Access from Port Wakefield Road will be limited to left in only with additional two-way access available on Hodgson Road via the internal road network.

The Department supports the proposed access arrangements, noting that all access to/from the development will be gained via the existing internal road network. Proposed and existing wayfinding signage throughout the site should reinforce the desired traffic flow and ensure continued effective operation of the existing access points on Port Wakefield Road and Hodgson Road.

The Traffic Report produced by GTA Consultants (Ref No. DA361/778/20, dated 15 December 2020), demonstrates that the proposed development would likely generate approximately 150 additional peak hour movements which would be easily accommodated within the adjacent road network. The GTA report identifies that the drive-through component of the development includes sufficient queuing capacity to ensure vehicles do not queue back onto Port Wakefield Road. The existing access arrangements to/from the site via Port Wakefield Road and Hodgson Road are expected to cater for the largest vehicle expected to access the site (11.6 metre rigid truck).

#16482382

Council should be satisfied that the access and any on-site vehicle manoeuvring areas remain clear of any impediments and that all off-street car parking areas be designed in accordance with AS/NZS 2890.1:2004 and AS/NZS 2890.6:2009.

It is recommended that any proposed signage is consistent with DIT's publication 'Advertising Signs: Assessment Guidelines for Road Safety'.

ADVICE

DIT supports the proposed development. The planning authority is advised to attach the following conditions to any approval:

1. All vehicular access from Port Wakefield Road shall in accordance with the Site Plan produced by Richmond + Ross Consulting Engineers and Project Leaders (Drawing No. A041, dated 12 August 2020). All additional access and traffic controls shall be in accordance with the site plan produced by GTA Consultants titled 'Traffic Management for Access' (Ref No. DA361/778/20, dated 15 December 2020).
2. All vehicles shall enter and exit the site in a forward direction.
3. The entry and exit points shall be suitably signed and line-marked to reinforce the desired traffic flow.
4. Stormwater run-off shall be collected on-site and discharged without impacting the integrity and safety of the adjacent road network. Any alterations to the road drainage infrastructure required to facilitate this shall be at the applicant's cost.

The following note provides important information for the benefit of the applicant and are required to be included in any approval:

- This property abuts a section of Port Wakefield Road that was proclaimed a controlled-access road on 13 June 1968 pursuant to Part 2A of the Highways Act 1926. The Department has authorised the use of the existing access point on Port Wakefield to service this site.

Yours sincerely



MANAGER, TRANSPORT ASSESSMENT
for **COMMISSIONER OF HIGHWAYS**

A copy of the decision notification form should be forwarded to dpti.developmentapplications@sa.gov.au

##16482382

Attachment 4
Extract of Relevant Development
Plan Provisions and Location Maps
(Consolidated 4 April 2019)



Salisbury Council

Consolidated - 4 April 2019

Please refer to the Salisbury Council page at www.sa.gov.au/developmentplans to see any amendments not consolidated.



Government of South Australia
Department of Planning,
Transport and Infrastructure

Consolidated - 4 April 2019

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Advertisements

OBJECTIVES

- 1 Urban and rural landscapes that are not disfigured by advertisements and/or advertising hoardings.
- 2 Advertisements and/or advertising hoardings that do not create a hazard.
- 3 Advertisements and/or advertising hoardings designed to enhance the appearance of the building and locality.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 The location, siting, design, materials, size, and shape of advertisements and/or advertising hoardings should be:
 - (a) consistent with the predominant character of the urban or rural landscape
 - (b) in harmony with any buildings or sites of historic significance or heritage value in the area
 - (c) co-ordinated with and complement the architectural form and design of the building they are to be located on.
- 2 The number of advertisements and/or advertising hoardings associated with a development should be minimised to avoid:
 - (a) clutter
 - (b) disorder
 - (c) untidiness of buildings and their surrounds
 - (d) driver distraction.
- 3 Buildings occupied by a number of tenants should exhibit co-ordinated and complementary advertisements and/or advertising hoardings to identify the tenants and their type of business.
- 4 The content of advertisements should be limited to information relating to the legitimate use of the associated land.
- 5 Advertisements and/or advertising hoardings should:
 - (a) be completely contained within the boundaries of the subject allotment
 - (b) be sited to avoid damage to, or pruning or lopping of, on-site landscaping or street trees
 - (c) not obscure views to vistas or objects of high amenity value.
- 6 Advertisements and/or advertising hoardings should not be erected on:
 - (a) a public footpath or veranda post
 - (b) a road, median strip or traffic island
 - (c) a vehicle adapted and exhibited primarily as an advertisement

Salisbury Council
General Section
Advertisements

- (d) residential land.
- 7 Advertisements and/or advertising hoardings attached to buildings should not be sited on the roof or higher than the walls of a building.
- 8 Advertisements and/or advertising hoardings erected on a veranda or that project from a building wall should:
- have a clearance over a footway, of at least 2.5 metres, to allow for safe and convenient pedestrian access
 - where erected on the side of a veranda, not exceed the width of the veranda or project from the veranda
 - where erected on the front of a veranda, not exceed the length of the veranda or project from the veranda
 - where projecting from a wall, have the edge of the advertisement or advertising hoarding abutting the surface of the wall.
- 9 Advertisements should be designed to conceal their supporting advertising hoarding from view.
- 10 Advertisements should convey the owner/occupier and/or generic type of business, merchandise or services using simple, clear and concise language, symbols, print style and layout and a small number of colours.
- 11 Advertisements which perform a secondary role in identifying the business, goods or services should only be readable in the immediate vicinity of the site.
- 12 Outside of townships and country settlements advertisements other than traffic signs, tourist signs or advertisements on an existing tourist information bay display board, should not be erected in road reserves.

Safety

- 13 Advertisements and/or advertising hoardings should not create a hazard by:
- being so highly illuminated as to cause discomfort to an approaching driver, or to create difficulty in the driver's perception of the road or persons or objects on the road
 - being liable to interpretation by drivers as an official traffic sign, or convey to drivers information that might be confused with instructions given by traffic signals or other control devices, or impair the conspicuous nature of traffic signs or signals
 - distracting drivers from the primary driving task at a location especially where the demands on driver concentration are high
 - obscuring a driver's view of other road or rail vehicles at/or approaching level crossings, or of pedestrians or of features of the road that are potentially hazardous (eg junctions, bends, changes in width, traffic control devices).
- 14 Any internally illuminated advertising signs and/or advertising hoardings which utilise LED, LCD or other similar technologies should be located a minimum of 80 metres from traffic signals, level crossings and other important traffic control devices.

Freestanding Advertisements

- 15 Freestanding advertisements and/or advertising hoardings should be:
- limited to only one primary advertisement per site or complex

(b) of a scale and size in keeping with the desired character of the locality and compatible with the development on the site.

- 16 Freestanding advertisements and/or advertising hoardings for multiple-business tenancy complexes should:
- incorporate the name or nature of each business or activity within the site or complex in a single advertisement
 - be integrally designed and mounted below the more predominant main complex or site identity advertisement.
- 17 Portable, easel or A-frame advertisements should be displayed only where:
- no other appropriate opportunity exists for an adequate co-ordinated and permanently erected advertisement and/or advertising hoarding
 - they do not obstruct or compromise the safety of pedestrians or vehicle movement
 - there is no unnecessary duplication or proliferation of advertising information
 - there is no damage to, or removal of, any landscaping on the site
 - they are restricted to 1 per site, or 1 per major road frontage if located upon a large corner site
 - each sign does not exceed 1 square metre in advertisement area per face, and 1.2 metres in height.
- 18 Freestanding advertisements should not exceed the total height (measured from natural ground level) as specified within the following table:

Location of freestanding advertisement	Total height (in metres)
Mixed Use (Bulky Goods, Entertainment and Leisure) Zone Precinct 8 Retail Core (within the Ingle Farm Policy Area 2) Core area within the Urban Core (Salisbury) Zone Precinct 17 Retail Core (within the Salisbury Downs Policy Area 4) Bulky Goods Zone Precinct 23 Greenfields Commercial (within the Commercial Zone)	8
Industry Zone Neighbourhood Centre Zone Precinct 21 Para Hills West Commercial (within the Commercial Zone) Precinct 24 Pooraka Commercial (within the Commercial Zone)	6
Commercial Zone (Except within Precinct 23 Greenfields Commercial , Precinct 21 Para Hills West Commercial or Precinct 24 Pooraka Commercial) Precinct 5 Education (within the Ingle Farm Policy Area 2) Precinct 7 Recreation (within the Ingle Farm Policy Area 2) Local Centre Zone Precinct 15 Community (within the Salisbury Downs Policy Area 4) Precinct 16 Mixed Use (within the Salisbury Downs Policy Area 4) Transition area within the Urban Core (Salisbury) Zone	4
In all other locations	3

- 19 Portable, easel or A-frame advertisements associated with a development should be displayed only during the hours the development is open for trading.

Salisbury Council
General Section
Advertisements

Flags, Bunting and Streamers

- 20 Advertisements and/or advertising hoardings incorporating any flags, bunting, streamers, or suspended objects should:
- (a) be placed or arranged to complement and accord with the scale of the associated development
 - (b) other than flags, not be positioned higher than the building they are attached or related to
 - (c) not be displayed in residential areas.

Advertising along Arterial Roads

- 21 Advertising and/or advertising hoardings should not be placed along arterial roads that have a speed limit of 80 km/h or more.

Salisbury Council
General Section
Centres and Retail Development

Centres and Retail Development

OBJECTIVES

- 1 Shopping, administrative, cultural, community, entertainment, educational, religious and recreational facilities located in integrated centres.
- 2 Centres that ensure rational, economic and convenient provision of goods and services and provide:
 - (a) a focus for community life
 - (b) safe, permeable, pleasant and accessible walking and cycling networks.
- 3 The provision of a safe pedestrian environment within centres which gives high priority to pedestrians, public and community transport.
- 4 Increased vitality and activity in centres through the introduction and integration of housing.
- 5 Centres developed in accordance with a hierarchy based on function, so that each type of centre provides a proportion of the total requirement of goods and services commensurate with its role.
- 6 Development of centres outside of Greater Adelaide in accordance with the following hierarchy:
 - (a) Regional Centre
 - (b) District Centre
 - (c) Town Centre (for smaller towns with a single centre zone)
 - (d) Local Centre (subsidiary centres for towns with a regional or district centre).
- 7 The central business district of the City of Adelaide providing the principal focus for the economic, social and political life of Greater Adelaide and the State.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development within centres should:
 - (a) integrate facilities within the zone
 - (b) allow for the multiple use of facilities and the sharing of utility spaces
 - (c) allow for the staging of development within the centre
 - (d) be integrated with public and community transport.
- 2 Development within centres should be designed to be compatible with adjoining areas. This should be promoted through landscaping, screen walls, centre orientation, location of access ways, buffer strips and transitional use areas.
- 3 Development within centres should provide:
 - (a) public spaces such as malls, plazas and courtyards

- (b) street furniture, including lighting, signs, litter bins, seats and bollards, that is sited and designed to complement the desired character
 - (c) unobtrusive facilities for the storage and removal of waste materials
 - (d) public facilities including toilets, infant changing facilities for parents, seating, litter bins, telephones and community information boards
 - (e) access for public and community transport and sheltered waiting areas for passengers
 - (f) lighting for pedestrian paths, buildings and associated areas
 - (g) a single landscaping theme
 - (h) safe and secure bicycle parking
- 4 Development should be designed to minimise energy consumption for lighting, heating, cooling and ventilation.
- 5 A single architectural theme should be established within centres through:
- (a) constructing additions or other buildings in a style complementary to the existing shopping complex
 - (b) renovating the existing shopping complex to complement new additions and other buildings within the centre
 - (c) employing a signage theme.
- 6 The design of undercroft or semi-basement car parking areas should not detract from the visual quality and amenity of adjacent pedestrian paths, streets or public spaces.
- 7 Undercroft or semi-basement car parking areas should not project above natural or finished ground level by more than 1 metre.

Arterial Roads

- 8 Centres should develop on one side of an arterial road or in one quadrant of an arterial road intersection.
- 9 Centre development straddling an arterial road should:
- (a) concentrate on one side of the arterial road or one quadrant of the arterial road intersection
 - (b) minimise the need for pedestrian and vehicular movement from one part of the centre to another across the arterial road.

Retail Development

- 10 A shop or group of shops with a gross leaseable area of greater than 250 square metres should be located within a centre zone.
- 11 A shop or group of shops with a gross leaseable area of less than 250 square metres should not be located on arterial roads unless within a centre zone.
- 12 A shop or group of shops located outside of zones that allow for retail development should:
- (a) be of a size and type that will not hinder the development, function or viability of any centre zone
 - (b) not demonstrably lead to the physical deterioration of any designated centre

Salisbury Council
General Section
Centres and Retail Development

- (c) be developed taking into consideration its effect on adjacent development.
- 13 Bulky goods outlets should only be located in centres, **commercial** and bulky goods zones.
- 14 Bulky goods outlets located within centres zones should:
 - (a) complement the overall provision of facilities
 - (b) be sited towards the periphery of those centres where the bulky goods outlet has a gross leaseable area of **500** square metres or more.

Crime Prevention

OBJECTIVES

- 1 A safe, secure, crime resistant environment where land uses are integrated and designed to facilitate community surveillance.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be designed to maximise surveillance of public spaces through the incorporation of clear lines of sight, appropriate lighting and the use of visible permeable barriers wherever practicable.
- 2 Buildings should be designed to overlook public and communal streets and public open space to allow casual surveillance.
- 3 Development should provide a robust environment that is resistant to vandalism and graffiti.
- 4 Development should provide lighting in frequently used public spaces including those:
 - (a) along dedicated cyclist and pedestrian pathways, laneways and access routes
 - (b) around public facilities such as toilets, telephones, bus stops, seating, litter bins, automatic teller machines, taxi ranks and car parks.
- 5 Development, including car park facilities should incorporate signage and lighting that indicate the entrances and pathways to, from and within sites.
- 6 Landscaping should be used to assist in discouraging crime by:
 - (a) screen planting areas susceptible to vandalism
 - (b) planting trees or ground covers, rather than shrubs, alongside footpaths
 - (c) planting vegetation other than ground covers a minimum distance of two metres from footpaths to reduce concealment opportunities.
- 7 Site planning, buildings, fences, landscaping and other features should clearly differentiate public, communal and private areas.
- 8 Buildings should be designed to minimise and discourage access between roofs, balconies and windows of adjoining dwellings.
- 9 Public toilets should be located, sited and designed:
 - (a) to promote the visibility of people entering and exiting the facility (eg by avoiding recessed entrances and dense shrubbery that obstructs passive surveillance)
 - (b) near public and community transport links and pedestrian and cyclist networks to maximise visibility.
- 10 Development should avoid pedestrian entrapment spots and movement predictors (eg routes or paths that are predictable or unchangeable and offer no choice to pedestrians).

Salisbury Council
General Section
Crime Prevention

11. Development should be designed to maximise surveillance of open space, pedestrian routes, centres and residential areas by:
- (a) orienting the frontages and entrances of buildings towards the public street
 - (b) avoiding screens, high walls, carports and landscaping that obscure direct views to public areas
 - (c) placing the entrances of buildings opposite each other across a street, or group entrances of multiple dwelling developments onto a commonly visible area to provide maximum mutual surveillance
 - (d) arranging living areas, windows, access ways and balconies to overlook open space and recreation areas and provide observation points to all areas of a site, particularly entrances and car parks.

Design and Appearance

OBJECTIVES

- 1 Development of a high architectural standard that responds to and reinforces positive aspects of the local environment and built form.
- 2 Roads, open spaces, buildings and land uses laid out and linked so that they are easy to understand and navigate.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 The design of a building may be of a contemporary nature and exhibit an innovative style provided the overall form is sympathetic to the scale of development in the locality and with the context of its setting with regard to shape, size, materials and colour.
- 2 Buildings should be designed and sited to avoid creating extensive areas of uninterrupted walling facing areas exposed to public view.
- 3 Buildings should be designed to reduce their visual bulk and provide visual interest through design elements such as:
 - (a) articulation
 - (b) colour and detailing
 - (c) small vertical and horizontal components
 - (d) design and placing of windows
 - (e) variations to facades.
- 4 Where a building is sited on or close to a side boundary, the side boundary wall should be sited and limited in length and height to minimise:
 - (a) the visual impact of the building as viewed from adjoining properties
 - (b) overshadowing of adjoining properties and allow adequate sun light to neighbouring buildings.
- 5 Building form should not unreasonably restrict existing views available from neighbouring properties and public spaces.
- 6 Transportable buildings and buildings which are elevated on stumps, posts, piers, columns or the like, should have their suspended footings enclosed around the perimeter of the building with brickwork or timber, and the use of verandas, pergolas and other suitable architectural detailing to give the appearance of a permanent structure.
- 7 The external walls and roofs of buildings should not incorporate highly reflective materials which will result in glare to neighbouring properties or drivers.
- 8 Structures located on the roofs of buildings to house plant and equipment should form an integral part of the building design in relation to external finishes, shaping and colours.
- 9 Building design should emphasise pedestrian entry points to provide perceptible and direct access from public street frontages and vehicle parking areas.

Salisbury Council
General Section
Design and Appearance

- 10 Development should provide clearly recognisable links to adjoining areas and facilities.
- 11 Buildings, landscaping, paving and signage should have a co-ordinated appearance that maintains and enhances the visual attractiveness of the locality.
- 12 Buildings (other than ancillary buildings or group dwellings) should be designed so that their main façade faces the primary street frontage of the land on which they are situated.
- 13 Where applicable, development should incorporate verandas over footpaths to enhance the quality of the pedestrian environment.
- 14 Development should be designed and sited so that outdoor storage, loading and service areas are screened from public view by an appropriate combination of built form, solid fencing and/or landscaping.
- 15 Outdoor lighting should not result in light spillage on adjacent land.
- 16 Balconies should:
 - (a) be integrated with the overall architectural form and detail of the building
 - (b) be sited to face predominantly north, east or west to provide solar access
 - (c) have a minimum area of 2 square metres.

Development Adjacent Heritage Places

- 17 The design of multi-storey buildings should not detract from the form and materials of adjacent State and local heritage places listed in [Table Sal/4 - State Heritage Places](#).
- 18 Development on land adjacent to a State or local heritage place, as listed in [Table Sal/4 - State Heritage Places](#) should be sited and designed to reinforce the historic character of the place and maintain its visual prominence.

Overshadowing

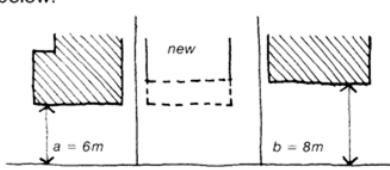
- 19 The design and location of buildings should enable direct winter sunlight into adjacent dwellings and private open space and minimise the overshadowing of:
 - (a) windows of habitable rooms
 - (b) upper-level private balconies that provide the primary open space area for a dwelling
 - (c) solar collectors (such as solar hot water systems and photovoltaic cells).

Visual Privacy

- 20 Development should minimise direct overlooking of habitable rooms and private open spaces of dwellings through measures such as:
 - (a) off-setting the location of balconies and windows of habitable rooms with those of other buildings so that views are oblique rather than direct
 - (b) building setbacks from boundaries (including building boundary to boundary where appropriate) that interrupt views or that provide a spatial separation between balconies or windows of habitable rooms
 - (c) screening devices (including fencing, obscure glazing, screens, external ventilation blinds, window hoods and shutters) that are integrated into the building design and have minimal negative effect on residents' or neighbours' amenity.
- 21 Permanently fixed external screening devices should be designed and coloured to complement the associated building's external materials and finishes

Building Setbacks from Road Boundaries

- 22 The setback of buildings from public roads should:
 - (a) be similar to, or compatible with, setbacks of buildings on adjoining land and other buildings in the locality
 - (b) contribute positively to the streetscape character of the locality
 - (c) not result in or contribute to a detrimental impact upon the function, appearance or character of the locality.
- 23 Except where specified in a particular zone, policy area or precinct, the main face of a building should be set back from the primary road frontage in accordance with the following table:

Setback difference between buildings on adjacent allotments	Setback of new building
Up to 2 metres	The same setback as one of the adjacent buildings, as illustrated below:  <p style="text-align: center;"><i>When $b > a \geq 2$, setback of new dwelling = a or b</i></p>
Greater than 2 metres	At least the average setback of the adjacent buildings.

- 24 Except where specified in a particular zone, policy area, or precinct, buildings and structures should be set back from road boundaries having regard to the requirements set out in Table Sal/1 - Building Setbacks from Road Boundaries.
- 25 Except where specified in a zone, policy area or precinct, the setback of development from a secondary street frontage should reflect the setbacks of the adjoining buildings and other buildings in the locality.
- 26 Development likely to encroach within a road widening setback under the *Metropolitan Adelaide Road Widening Plan Act 1972* should be set back sufficiently from the boundary required for road widening.

Salisbury Council
General Section
Energy Efficiency

Energy Efficiency

OBJECTIVES

- 1 Development designed and sited to conserve energy.
- 2 Development that provides for on-site power generation including photovoltaic cells and wind power.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should provide for efficient solar access to buildings and open space all year around.
- 2 Buildings should be sited and designed:
 - (a) to ensure adequate natural light and winter sunlight is available to the main activity areas of adjacent buildings
 - (b) so that open spaces associated with the main activity areas face north for exposure to winter sun.

On-site Energy Generation

- 3 Development should facilitate the efficient use of photovoltaic cells and solar hot water systems by:
 - (a) taking into account overshadowing from neighbouring buildings
 - (b) designing roof orientation and pitches to maximise exposure to direct sunlight.
- 4 Public infrastructure and lighting, should be designed to generate and use renewable energy.

Hazards

OBJECTIVES

- 1 Maintenance of the natural environment and systems by limiting development in areas susceptible to natural hazard risk.
- 2 Development located away from areas that are vulnerable to, and cannot be adequately and effectively protected from the risk of natural hazards.
- 3 Critical community facilities such as hospitals, emergency control centres, major service infrastructure facilities, and emergency service facilities located where they are not exposed to natural hazard risks.
- 4 Development located and designed to minimise the risks to safety and property from flooding.
- 5 Development located to minimise the threat and impact of bushfires on life and property.
- 6 Expansion of existing non-rural uses directed away from areas of high bushfire risk.
- 7 The environmental values and ecological health of receiving waterways and marine environments protected from the release of acid water resulting from the disturbance of acid sulphate soils.
- 8 Protection of human health and the environment wherever site contamination has been identified or is suspected to have occurred.
- 9 Appropriate assessment and remediation of site contamination to ensure land is suitable for the proposed use and provides a safe and healthy living and working environment.
- 10 Minimisation of harm to life, property and the environment through appropriate location of development and appropriate storage, containment and handling of hazardous materials.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be excluded from areas that are vulnerable to, and cannot be adequately and effectively protected from, the risk of hazards.
- 2 Development located on land subject to hazards as shown on the *Overlay Maps - Development Constraints* should not occur unless it is sited, designed and undertaken with appropriate precautions being taken against the relevant hazards.
- 3 There should not be any significant interference with natural processes in order to reduce the exposure of development to the risk of natural hazards.

Flooding

- 4 Development should not occur on land where the risk of flooding is likely to be harmful to safety or damage property.
- 5 Development should not be undertaken in areas liable to inundation by tidal, drainage or flood waters unless the development can achieve all of the following:
 - (a) it is developed with a public stormwater system capable of catering for a 1-in-100 year average return interval flood event
 - (b) buildings are designed and constructed to prevent the entry of floodwaters in a 1-in-100 year average return interval flood event.

Salisbury Council
General Section
Hazards

- 6 Development, including earthworks associated with development, should not do any of the following:
- (a) impede the flow of floodwaters through the land or other surrounding land
 - (b) increase the potential hazard risk to public safety of persons during a flood event
 - (c) aggravate the potential for erosion or siltation or lead to the destruction of vegetation during a flood
 - (d) cause any adverse effect on the floodway function
 - (e) increase the risk of flooding of other land
 - (f) obstruct a watercourse.

Bushfire

- 7 The following bushfire protection principles of development control apply to development of land identified as General, Medium and High bushfire risk areas as shown on the *Bushfire Protection Area BPA Maps - Bushfire Risk*.
- 8 Development in a Bushfire Protection Area should be in accordance with those provisions of the *Minister's Code: Undertaking development in Bushfire Protection Areas* that are designated as mandatory for Development Plan Consent purposes.
- 9 Buildings and structures should be located away from areas that pose an unacceptable bushfire risk as a result of one or more of the following:
- (a) vegetation cover comprising trees and/or shrubs
 - (b) poor access
 - (c) rugged terrain
 - (d) inability to provide an adequate building protection zone
 - (e) inability to provide an adequate supply of water for fire-fighting purposes.
- 10 Residential, tourist accommodation and other habitable buildings should:
- (a) be sited on the flatter portion of allotments and avoid steep slopes, especially upper slopes, narrow ridge crests and the tops of narrow gullies, and slopes with a northerly or westerly aspect
 - (b) be sited in areas with low bushfire hazard vegetation and set back at least 20 metres from existing hazardous vegetation
 - (c) have a dedicated and accessible water supply available at all times for fire fighting.
- 11 Extensions to existing buildings, outbuildings and other ancillary structures should be sited and constructed using materials to minimise the threat of fire spread to residential, tourist accommodation and other habitable buildings in the event of bushfire.
- 12 Buildings and structures should be designed and configured to reduce the impact of bushfire through using simple designs that reduce the potential for trapping burning debris against the building or structure, or between the ground and building floor level in the case of transportable buildings.
- 13 Land division for residential or tourist accommodation purposes within areas of high bushfire risk should be limited to those areas specifically set aside for these uses.

- 14 Where land division does occur it should be designed to:
- minimise the danger to residents, other occupants of buildings and fire fighting personnel
 - minimise the extent of damage to buildings and other property during a bushfire
 - ensure each allotment contains a suitable building site that is located away from vegetation that would pose an unacceptable risk in the event of bushfire
 - ensure provision of a fire hazard separation zone isolating residential allotments from areas that pose an unacceptable bushfire risk by containing the allotments within a perimeter road or through other means that achieve an adequate separation.
- 15 Vehicle access and driveways to properties and public roads created by land division should be designed and constructed to:
- facilitate safe and effective operational use for fire-fighting and other emergency vehicles and residents
 - provide for two-way vehicular access between areas of fire risk and the nearest public road.
- 16 Olive orchards should be located and developed in a manner that minimises their potential to fuel bushfires.

Salinity

- 17 Development should not increase the potential for, or result in an increase in, soil and water salinity.
- 18 Preservation, maintenance and restoration of locally indigenous plant species should be encouraged in areas affected by dry land salinity.
- 19 Irrigated horticulture and pasture should not increase groundwater-induced salinity.

Acid Sulfate Soils

- 20 Development and activities, including excavation and filling of land, that may lead to disturbance of potential or actual acid sulfate soils (including land identified on the *Overlay Maps – Development Constraints*) should be avoided unless such disturbances are managed in a way that effectively avoids the potential for harm or damage to any of the following:
- the marine and estuarine environment
 - natural water bodies and wetlands
 - agricultural or aquaculture activities
 - buildings, structures and infrastructure
 - public health.
- 21 Development, including primary production, aquaculture activities and infrastructure, should not proceed unless it can be demonstrated that the risk of releasing acid water resulting from the disturbance of acid sulfate soils is minimal.

Site Contamination

- 22 Development, including land division, should not occur where site contamination has occurred unless the site has been assessed and remediated as necessary to ensure that it is suitable and safe for the proposed use.

Salisbury Council
General Section
Hazards

Containment of Chemical and Hazardous Materials

- 23 Hazardous materials should be stored and contained in a manner that minimises the risk to public health and safety and the potential for water, land or air contamination.
- 24 Development that involves the storage and handling of hazardous materials should ensure that these are contained in designated areas that are secure, readily accessible to emergency vehicles, impervious, protected from rain and stormwater intrusion and other measures necessary to prevent:
 - (a) discharge of polluted water from the site
 - (b) contamination of land
 - (c) airborne migration of pollutants
 - (d) potential interface impacts with sensitive land uses.

Landslip

- 25 Development, including associated cut and fill activities, should not lead to an increased danger from land surface instability or to the potential of landslip occurring on the site or on surrounding land.
- 26 Development on steep slopes should promote the retention and replanting of vegetation as a means of stabilising and reducing the possibility of surface movement or disturbance.
- 27 Development in areas susceptible to landslip should:
 - (a) incorporate split level designs to minimise cutting into the slope
 - (b) ensure that cut and fill and heights of faces are minimised
 - (c) ensure cut and fill is supported with engineered retaining walls or are battered to appropriate grades
 - (d) control any erosion that will increase the gradient of the slope and decrease stability
 - (e) ensure the siting and operation of an effluent drainage field does not contribute to landslip
 - (f) provide drainage measures to ensure surface stability is not compromised
 - (g) ensure natural drainage lines are not obstructed.

Infrastructure

OBJECTIVES

- 1 Infrastructure provided in an economical and environmentally sensitive manner.
- 2 Infrastructure, including social infrastructure, provided in advance of need.
- 3 Suitable land for infrastructure identified and set aside in advance of need.
- 4 The visual impact of infrastructure facilities minimised.
- 5 The efficient and cost-effective use of existing infrastructure.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not occur without the provision of adequate utilities and services, including:
 - (a) electricity supply
 - (b) water supply
 - (c) drainage and stormwater systems
 - (d) waste disposal
 - (e) effluent disposal systems
 - (f) formed all-weather public roads
 - (g) telecommunications services
 - (h) social infrastructure, community services and facilities
 - (i) gas services.
- 2 Development should only occur where it provides, or has access to, relevant easements for the supply of infrastructure.
- 3 Development should incorporate provision for the supply of infrastructure services to be located within common service trenches where practicable.
- 4 Development should not take place until adequate and co-ordinated drainage of the land is assured.
- 5 Development in urban areas should not occur without provision of an adequate reticulated domestic quality mains water supply and an appropriate waste treatment system.
- 6 In areas where no reticulated water supply is available, buildings whose usage is reliant on a water supply should be equipped with an adequate and reliable on-site water storage system.
- 7 Electricity infrastructure should be designed and located to minimise its visual and environmental impacts.
- 8 Development and landscaping within 25 metres of the 275 kV overhead electricity lines should ensure that all clearances and safety restrictions are met.

Salisbury Council
General Section
Infrastructure

- 9 In urban areas, electricity supply serving new development should be installed underground.
- 10 Utilities and services, including access roads and tracks, should be sited on areas already cleared of native vegetation. If this is not possible, their siting should cause minimal interference or disturbance to existing native vegetation and biodiversity.
- 11 Utility buildings and structures should be grouped with non-residential development where possible.
- 12 Development in proximity to infrastructure facilities should be sited and be of a scale to ensure adequate separation to protect people and property.

Interface between Land Uses

OBJECTIVES

- 1 Development located and designed to minimise adverse impact and conflict between land uses.
- 2 Protect community health and amenity from adverse impacts of development.
- 3 Protect desired land uses from the encroachment of incompatible development.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:
 - (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants
 - (b) noise
 - (c) vibration
 - (d) electrical interference
 - (e) light spill
 - (f) glare
 - (g) hours of operation
 - (h) traffic impacts.
- 2 Development should be sited and designed to minimise negative impacts on existing and potential future land uses desired in the locality.
- 3 Development adjacent to a **Residential Zone** should be designed to minimise overlooking and overshadowing of adjacent dwellings and private open space.
- 4 Residential development adjacent to non-residential zones and land uses should be located, designed and/or sited to protect residents from potential adverse impacts from non-residential activities.
- 5 Sensitive uses likely to conflict with the continuation of lawfully existing developments and land uses desired for the zone should be designed to minimise negative impacts.
- 6 Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.

Noise Generating Activities

- 7 Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant *Environment Protection (Noise) Policy* criteria when assessed at the nearest existing noise sensitive premises.
- 8 Development with the potential to emit significant noise (e.g. industry) should incorporate noise attenuation measures that prevent noise from causing unreasonable interference with the amenity of noise sensitive premises.

Salisbury Council
General Section
Interface between Land Uses

- 9 Outdoor areas (such as beer gardens or dining areas) associated with licensed premises should be designed or sited to minimise adverse noise impacts on adjacent existing or future noise sensitive development.
- 10 Development proposing music should include noise attenuation measures that achieve the following desired noise levels:

Noise level assessment location	Desired noise level
Adjacent existing <i>noise sensitive development</i> property boundary	Less than 8 dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum and Less than 5 dB(A) above the level of background noise ($LA_{90,15min}$) for the overall (sum of all octave bands) A-weighted level
Adjacent <i>land</i> property boundary	Less than 65dB(Lin) at 63Hz and 70dB(Lin) in all other octave bands of the sound spectrum or Less than 8 dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum and 5 dB(A) overall (sum of all octave bands) A-weighted level

Air Quality

- 11 Development with the potential to emit harmful or nuisance-generating air pollution should incorporate air pollution control measures to prevent harm to human health or unreasonable interference with the amenity of sensitive uses within the locality.
- 12 Chimneys or exhaust flues associated with commercial development (including cafes, restaurants and fast food outlets) should be designed to ensure they do not cause a nuisance or health concerns to nearby sensitive receivers by:
- incorporating appropriate treatment technology before exhaust emissions are released to the atmosphere
 - ensuring that the location and design of chimneys or exhaust flues maximises dispersion and takes into account the location of nearby sensitive uses.

Rural Interface

- 13 The potential for adverse impacts resulting from rural development should be minimised by:
- not locating horticulture or intensive animal keeping on land adjacent to townships
 - maintaining an adequate separation between horticulture or intensive animal keeping and townships, other sensitive uses and, where desirable, other forms of primary production.
- 14 Traffic movement, spray drift, dust, noise, odour and the use of frost fans and gas guns associated with primary production should not lead to unreasonable impact on adjacent land uses.
- 15 Existing primary production and mineral extraction should not be prejudiced by the inappropriate encroachment of sensitive uses such as urban development.
- 16 Development that is adjacent to land used for primary production (within either the zone or adjacent zones) should include appropriate setbacks and vegetative plantings designed to minimise the potential impacts of chemical spray drift and other impacts associated with primary production.

Salisbury Council
General Section
Interface between Land Uses

- 17 New urban development should provide a buffer of at least 40 metres wide (inclusive of any fuel break, emergency vehicle access or road) separating urban and rural activities.
- 18 Development located within 300 metres of facilities for the handling, transportation and storage of bulk commodities should:
 - (a) not prejudice the continued operation of those facilities
 - (b) be located, designed and developed having regard to the potential environmental impact arising from the operation of such facilities and the potential extended hours of operation.

Landscaping, Fences and Walls

OBJECTIVES

- 1 The amenity of land and development enhanced with appropriate planting and other landscaping works, using locally indigenous plant species where possible.
- 2 Functional fences and walls that enhance the attractiveness of development.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should incorporate open space and landscaping and minimise hard paved surfaces in order to:
 - (a) complement built form and reduce the visual impact of larger buildings (eg taller and broader plantings against taller and bulkier building components)
 - (b) enhance the appearance of road frontages
 - (c) screen service yards, loading areas and outdoor storage areas
 - (d) minimise maintenance and watering requirements
 - (e) enhance and define outdoor spaces, including car parking areas
 - (f) maximise shade and shelter
 - (g) assist in climate control within and around buildings
 - (h) minimise heat absorption and reflection
 - (i) maintain privacy
 - (j) maximise stormwater re-use
 - (k) complement existing vegetation, including native vegetation
 - (l) contribute to the viability of ecosystems and species
 - (m) promote water and biodiversity conservation.
- 2 Landscaping should:
 - (a) include the planting of locally indigenous species where appropriate
 - (b) be oriented towards the street frontage
 - (c) result in the appropriate clearance from powerlines and other infrastructure being maintained.
- 3 Landscaping should not:
 - (a) unreasonably restrict solar access to adjoining development
 - (b) cause damage to buildings, paths and other landscaping from root invasion, soil disturbance or plant overcrowding

Salisbury Council
General Section
Landscaping, Fences and Walls

- (c) introduce pest plants
 - (d) increase the risk of bushfire
 - (e) remove opportunities for passive surveillance
 - (f) increase leaf fall in watercourses
 - (g) increase the risk of weed invasion
 - (h) obscure driver sight lines
 - (i) create a hazard for train or tram drivers by obscuring sight lines at crossovers.
- 4 Fences and walls, including retaining walls, should:
- (a) not result in damage to neighbouring trees
 - (b) be compatible with the associated development and with existing predominant, attractive fences and walls in the locality
 - (c) enable some visibility of buildings from and to the street to enhance safety and allow casual surveillance
 - (d) incorporate articulation or other detailing where there is a large expanse of wall facing the street
 - (e) assist in highlighting building entrances
 - (f) be sited and limited in height, to ensure adequate sight lines for motorists and pedestrians especially on corner sites
 - (g) in the case of side and rear boundaries, be of sufficient height to maintain privacy and/or security without adversely affecting the visual amenity or access to sunlight of adjoining land
 - (h) be constructed of non-flammable materials.

Salisbury Council
General Section
Metropolitan Open Space System

Metropolitan Open Space System

OBJECTIVES

- 1 A Metropolitan Open Space System (MOSS), in and around metropolitan Adelaide, that:
 - (a) is comprised of public and private land
 - (b) is clearly defined and linked
 - (c) has an open or natural character
 - (d) provides a visual and scenic contrast to the built urban environment
 - (e) separates different parts of the metropolitan area.
- 2 Conservation and restoration of existing and modified habitats.
- 3 Conservation of sites of scientific or heritage interest.
- 4 The provision of corridors for the movement of wildlife.
- 5 A range of recreation and leisure areas including a network of recreation trails integrating MOSS and adjoining land uses.
- 6 A range of active recreation and sporting facilities of regional or State significance, including facilities that can be used for national and international events.
- 7 Stormwater management in association with recreation, aquifer recharge and water quality management.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should preserve and enhance the natural and open character and amenity of land located within the MOSS.
- 2 Development within the MOSS should contribute to the rehabilitation and restoration of aquatic and terrestrial ecosystems and water catchments.
- 3 Landscaping within MOSS locations should:
 - (a) incorporate remnant vegetation
 - (b) use locally indigenous plant species wherever possible
 - (c) provide shade and windbreaks along cyclist and pedestrian routes and around picnic and barbecue areas, seating and car parking areas
 - (d) maximise opportunities for passive surveillance along the park.
- 4 Development on public land within the MOSS should incorporate:
 - (a) easily identified access points

Salisbury Council
General Section
Metropolitan Open Space System

- (b) pedestrian and cycle linkages and horse trails within open spaces and between other open space networks
 - (c) facilities to provide and support a range of recreation and leisure activities.
- 5 Buildings and structures erected on land within the MOSS should be designed, located and screened so as to be unobtrusive and not detract from the open natural or landscaped character of these areas

Salisbury Council
General Section
Open Space and Recreation

Open Space and Recreation

OBJECTIVES

- 1 The creation of a network of linked parks, reserves, recreational trails and recreation areas at regional and local levels.
- 2 Pleasant, functional and accessible open spaces providing a range of physical environments.
- 3 A wide range of settings for active and passive recreational opportunities.
- 4 An appropriate quantity, quality and distribution of open space that reflects the local, neighbourhood, district, council wide and regional needs of the community as reflected by indicators such as population density and demographic structure
- 5 The provision of open space in the following hierarchy:
 - State
 - Regional
 - District
 - Neighbourhood
 - Local.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Urban development should include public open space and recreation areas.
- 2 Public open space and recreation areas should be of a size, dimension and location that:
 - (a) facilitate a range of formal and informal recreation activities
 - (b) provide for the movement of pedestrians and cyclists
 - (c) incorporate existing vegetation and natural features, watercourses, wildlife habitat and other sites of natural or cultural value
 - (d) link habitats, wildlife corridors, public open spaces and existing recreation facilities
 - (e) enable effective stormwater management
 - (f) provides for the planting and retention of large trees and vegetation.
- 3 Open space should be designed to incorporate:
 - (a) pedestrian, cycle linkages to other open spaces, centres, schools and public transport nodes
 - (b) park furniture, shaded areas and resting places to enhance pedestrian comfort
 - (c) safe crossing points where pedestrian routes intersect the road network
 - (d) easily identified access points
 - (e) frontage to abutting public roads to optimise pedestrian access and visibility
 - (f) re-use of stormwater for irrigation purposes

- (g) recreational trails where appropriate
 - (h) opportunities to be active and participate in physical activity
 - (i) features that provide relief to the urban form.
- 4 Where practical, access points to regional parks should be located close to public transport.
 - 5 District level parks should be at least 3 hectares in size, and provided within 2 kilometres of all households that they serve.
 - 6 Neighbourhood parks should be at least 0.5 hectares and generally closer to 1 hectare in size, and provided within 500 metres of households that they serve.
 - 7 Local parks should be:
 - (a) a minimum of 0.3 hectares in size
 - (b) centrally located within a residential area, close to schools, shops and generally within 300 metres of households that they serve.
 - 8 No more than 20 per cent of land allocated as public open space should:
 - (a) have a slope in excess of 1-in-4
 - (b) comprise creeks or other drainage areas.
 - 9 Signage should be provided at entrances to and within public open space to provide clear orientation to major points of interest such as the location of public toilets, telephones, safe routes and park activities.
 - 10 Buildings in open space, including structures and associated car parking areas, should be designed, located and of a scale that is unobtrusive and does not detract from the desired open space character.
 - 11 Development in open space should:
 - (a) be clustered where practical to ensure that the majority of the site remains open
 - (b) where practical, be developed for multi-purpose use
 - (c) be constructed to minimise the extent of hard paved areas.
 - 12 Open spaces and recreation areas should be located and designed to maximise safety and security by:
 - (a) ensuring that within urban areas, their edges are overlooked by housing, commercial or other development that can provide effective informal surveillance
 - (b) ensuring fenced parks and playgrounds have more than one entrance or exit when fenced
 - (c) locating play equipment where it can be informally observed by nearby residents and users during times of use
 - (d) clearly defining the perimeters of play areas
 - (e) providing lighting around facilities such as toilets, telephones, seating, litter bins, bike storage and car parks
 - (f) focusing pedestrian and bicycle movement after dark along clearly defined, adequately lit routes with observable entries and exits.

Salisbury Council
General Section
Open Space and Recreation

- 13 Landscaping associated with open space and recreation areas should:
 - (a) not compromise the drainage function of any drainage channel
 - (b) provide shade and windbreaks along cyclist and pedestrian routes, around picnic and barbecue areas and seating, and in car parking areas
 - (c) maximise opportunities for informal surveillance throughout the park
 - (d) enhance the visual amenity of the area and complement existing buildings
 - (e) be designed and selected to minimise maintenance costs
 - (f) provide habitat for local fauna.
- 14 Development of recreational activities in areas not zoned for that purpose should be compatible with surrounding activities.
- 15 Recreation facilities development should be sited and designed to minimise negative impacts on the amenity of the locality.

Orderly and Sustainable Development

OBJECTIVES

- 1 Orderly and economical development that creates a safe, convenient and pleasant environment in which to live.
- 2 Development occurring in an orderly sequence and in a compact form to enable the efficient provision of public services and facilities.
- 3 Development that does not jeopardise the continuance of adjoining authorised land uses.
- 4 Development that does not prejudice the achievement of the provisions of the Development Plan.
- 5 Development abutting adjoining Council areas having regard to the policies of that Council's Development Plan.
- 6 Urban development contained within existing townships and settlements and located only in zones designated for such development.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not prejudice the development of a zone for its intended purpose.
- 2 Land outside of townships and settlements should primarily be used for primary production and conservation purposes.
- 3 The economic base of the region should be expanded in a sustainable manner.
- 4 Urban development should form a compact extension to an existing built-up area.
- 5 Ribbon development should not occur along the coast, water frontages or arterial roads shown in *Overlay Maps - Transport*.
- 6 Development should be located and staged to achieve the economical provision of public services and infrastructure, and to maximise the use of existing services and infrastructure.
- 7 Where development is expected to impact upon the existing infrastructure network (including the transport network), development should demonstrate how the undue effect will be addressed.
- 8 Vacant or underutilised land should be developed in an efficient and co-ordinated manner to not prejudice the orderly development of adjacent land.

Transportation and Access

OBJECTIVES

- 1 A comprehensive, integrated, affordable and efficient air, rail, sea, road, cycle and pedestrian transport system that will:
 - (a) provide equitable access to a range of public, community and private transport services for all people
 - (b) ensure a high level of safety
 - (c) effectively support the economic development of the State
 - (d) have minimal negative environmental and social impacts
 - (e) maintain options for the introduction of suitable new transport technologies.
- 2 Development that:
 - (a) provides safe and efficient movement for all motorised and non-motorised transport modes
 - (b) ensures access for vehicles including emergency services, public infrastructure maintenance and commercial vehicles
 - (c) provides off street parking
 - (d) is appropriately located so that it supports and makes best use of existing transport facilities and networks.
- 3 A road hierarchy that promotes safe and efficient transportation in an integrated manner throughout the State.
- 4 Provision of safe, pleasant, accessible, integrated and permeable pedestrian and cycling networks.
- 5 Safe and convenient freight movement throughout the State.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 Land uses arranged to support the efficient provision of sustainable transport networks and encourage their use.

Movement Systems

- 2 Development should be integrated with existing transport networks, particularly major rail and road corridors as shown on *Location Maps* and *Overlay Maps - Transport*, and designed to minimise its potential impact on the functional performance of the transport networks.
- 3 Transport corridors should be sited and designed so as to not unreasonably interfere with the health and amenity of adjacent sensitive land uses.
- 4 Roads should be sited and designed to blend with the landscape and be in sympathy with the terrain.

Salisbury Council
General Section
Transportation and Access

- 5 Land uses that generate large numbers of visitors such as shopping centres and areas, places of employment, schools, hospitals and medium to high density residential uses should be located so that they can be serviced by existing transport networks and encourage walking and cycling.
- 6 Development generating high levels of traffic, such as schools, shopping centres and other retail areas, entertainment and sporting facilities, should incorporate passenger pick-up and set down areas. The design of such areas should ensure interference to existing traffic is minimised and give priority to pedestrians, cyclists and public and community transport users.
- 7 The location and design of public and community transport set-down and pick-up points should maximise safety and minimise the isolation and vulnerability of users.
- 8 Development should provide safe and convenient access for all anticipated modes of transport including cycling, walking, public and community transport, and motor vehicles.
- 9 Development at intersections, pedestrian and cycle crossings, and crossovers to allotments should maintain or enhance sightlines for motorists, cyclists and pedestrians to ensure safety for all road users and pedestrians.
- 10 Driveway crossovers affecting pedestrian footpaths should maintain the level of the footpath.
- 11 Development should discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive land uses such as schools.
- 12 Industrial/commercial vehicle movements should be separated from passenger vehicle car-parking areas.
- 13 Development should make sufficient provision on site for the loading, unloading and turning of all traffic likely to be generated.

Cycling and Walking

- 14 Development should ensure that a permeable street and path network is established that encourages walking and cycling through the provision of safe, convenient and attractive routes with connections to adjoining streets, paths, open spaces, schools, public and community transport stops and activity centres.
- 15 Development should provide access, and accommodate multiple route options, for cyclists by enhancing and integrating with:
 - (a) open space networks, recreational trails, parks, reserves and recreation areas
 - (b) Adelaide's Metropolitan Open Space System.
- 16 Cycling and pedestrian networks should be designed to be permeable and facilitate direct and efficient passage to neighbouring networks and facilities.
- 17 New developments should give priority to and not compromise existing designated bicycle routes.
- 18 Where development coincides with, intersects or divides a proposed bicycle route or corridor, development should incorporate through-access for cyclists.
- 19 Developments should encourage and facilitate cycling as a mode of transport by incorporating end-of-journey facilities including:
 - (a) showers, changing facilities, and secure lockers
 - (b) signage indicating the location of bicycle facilities

- (c) secure bicycle parking facilities provided at the rate set out in [Table Sal/3 - Off Street Bicycle Parking Requirements](#).
- 20 Pedestrian facilities and networks should be designed and provided in accordance with relevant provisions of the *Australian Standards and Austroads Guide to Traffic Engineering Practice Part 13*.
- 21 Cycling facilities and networks should be designed and provided in accordance with the relevant provisions of the *Australian Standards and Austroads Guide to Traffic Engineering Practice Part 14*.

Access

- 22 Development should have direct access from an all weather public road.
- 23 Development should be provided with safe and convenient access which:
 - (a) avoids unreasonable interference with the flow of traffic on adjoining roads
 - (b) provides appropriate separation distances from existing roads or level crossings
 - (c) accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision
 - (d) is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.
- 24 Development should not restrict access to publicly owned land such as recreation areas.
- 25 The number of vehicle access points onto arterial roads shown on *Overlay Maps - Transport* should be minimised, and where possible access points should be:
 - (a) limited to local roads
 - (b) shared between developments.
- 26 The number of access points for cyclists and pedestrians onto all adjoining roads should be maximised.
- 27 Development with access from roads with existing or projected traffic volumes exceeding 6000 vehicles per day should be sited to avoid the need for vehicles to reverse on to or from the road.
- 28 Development with access from arterial roads or roads as shown on *Overlay Maps - Transport* should be sited to avoid the need for vehicles to reverse on to or from the road.
- 29 Driveways, access tracks and parking areas should be designed and constructed to:
 - (a) follow the natural contours of the land
 - (b) minimise excavation and/or fill
 - (c) minimise the potential for erosion from run-off
 - (d) avoid the removal of existing vegetation
 - (e) be consistent with *Australian Standard AS 2890 Parking facilities*.

Access for People with Disabilities

- 30 Development should be sited and designed to provide convenient access for people with a disability.
- 31 Where appropriate and practical, development should provide for safe and convenient access to the coast and beaches for disabled persons.

Salisbury Council
General Section
Transportation and Access

Vehicle Parking

- 32 Development should provide off-street vehicle parking and specifically marked disabled car parking places to meet anticipated demand in accordance with [Table Sal/2 - Off Street Vehicle Parking Requirements](#) or [Table Sal/2A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever applies) unless an agreement is reached between the Council and the applicant for a reduced number of parking spaces where one of the following applies:
- a financial contribution is paid into the Council Car Parking Funds specified by the Council, in accordance with the gazetted rate per car park associated with the 'Car Park Fund Areas' identified on [Concept Plan Map Sal/27 - Salisbury District Centre Car Park Fund Area](#), [Concept Plan Map Sal/29 - Ingle Farm District Centre Car Park Fund Area](#) and [Concept Plan Map Sal/32 - Mawson Lakes Town Centre Car Parking Fund Area](#)
 - it can be demonstrated that fewer car parks would be required to meet the car parking needs associated with the development.
- 33 Development should be consistent with [Australian Standard AS 2890 Parking facilities](#).
- 34 Vehicle parking areas should be sited and designed in a manner that will:
- facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development
 - include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network
 - not inhibit safe and convenient traffic circulation
 - result in minimal conflict between customer and service vehicles
 - avoid the necessity to use public roads when moving from one part of a parking area to another
 - minimise the number of vehicle access points to public roads
 - avoid the necessity for backing onto public roads
 - where reasonably possible, provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for access points
 - not dominate the character and appearance of a site when viewed from public roads and spaces
 - provide landscaping that will shade and enhance the appearance of the vehicle parking areas.
- 35 Vehicle parking areas should be designed to reduce opportunities for crime by:
- maximising the potential for passive surveillance by ensuring they can be overlooked from nearby buildings and roads
 - incorporating walls and landscaping that do not obscure vehicles or provide potential hiding places
 - being appropriately lit
 - having clearly visible walkways.
- 36 Where parking areas are not obviously visible or navigated, signs indicating the location and availability of vehicle parking spaces associated with businesses should be displayed at locations readily visible to customers.

- 37 Parking areas that are likely to be used during non daylight hours should provide floodlit entrance and exit points and site lighting directed and shaded in a manner that will not cause nuisance to adjacent properties or users of the car park.
- 38 Parking areas should be sealed or paved in order to minimise dust and mud nuisance.
- 39 To assist with stormwater detention and reduce heat loads in summer, vehicle parking areas should include soft (living) landscaping.
- 40 Parking areas should be line-marked to indicate parking bays, movement aisles and direction of traffic flow.

Vehicle Parking for Residential Development

- 41 On-site vehicle parking should be provided having regard to:
- (a) the number, nature and size of proposed dwellings
 - (b) proximity to centre facilities, public and community transport within walking distance of the dwellings
 - (c) the anticipated mobility and transport requirements of the likely occupants, particularly groups such as aged persons.
- 42 Vehicle parking areas servicing more than one dwelling should be of a size and location to:
- (a) serve users, including pedestrians, cyclists and motorists, efficiently, conveniently and safely
 - (b) provide adequate space for vehicles, including emergency service vehicles, to manoeuvre between the street and the parking area
 - (c) reinforce or contribute to attractive streetscapes.

Vehicle Parking for Mixed Use and Corridor Zones

- 43 Loading areas and designated parking spaces for service vehicles should:
- (a) be provided within the boundary of the site
 - (b) not be located in areas where there is parking provided for any other purpose.
- 44 Vehicle parking spaces and multi-level vehicle parking structures within buildings should:
- (a) enhance active street frontages by providing land uses such as commercial, retail or other non-car park uses along ground floor street frontages
 - (b) complement the surrounding built form in terms of height, massing and scale
 - (c) incorporate facade treatments along major street frontages that are sufficiently enclosed and detailed to complement neighbouring buildings consistent with the desired character of the locality.
- 45 In mixed use buildings, the provision of vehicle parking may be reduced in number and shared where the operating hours of commercial activities complement the residential use of the site.

Undercroft and Below Ground Garaging and Parking of Vehicles

- 46 Undercroft and below ground garaging of vehicles should only occur where envisaged in the relevant zone or policy area or precinct and ensure:
- (a) the overall height and bulk of the undercroft structure does not adversely impact on streetscape character of the locality or the amenity of adjacent properties

Salisbury Council
General Section
Transportation and Access

- (b) vehicles can safely enter and exit from the site without compromising pedestrian or cyclist safety or causing conflict with other vehicles
 - (c) driveway gradients provide for safe and functional entry and exit
 - (d) driveways and adjacent walls, fencing and landscaping are designed to provide adequate sightlines from vehicles to pedestrians using the adjacent footpath
 - (e) openings to undercroft areas are integrated with the main building so as to minimise visual impact
 - (f) landscaping, mounding and/or fencing is incorporated to improve its presentation to the street and to adjacent properties
 - (g) the overall streetscape character of the locality is not adversely impaired (e.g. visual impact, building bulk, front setbacks relative to adjacent development).
- 47 In the case of undercroft and below ground car parks where cars are visible from public areas, adequate screening and landscaping should be provided.

Waste

OBJECTIVES

- 1 Development that, in order of priority, avoids the production of waste, minimises the production of waste, reuses waste, recycles waste for reuse, treats waste and disposes of waste in an environmentally sound manner.
- 2 Development that includes the treatment and management of solid and liquid waste to prevent undesired impacts on the environment including, soil, plant and animal biodiversity, human health and the amenity of the locality.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be sited and designed to prevent or minimise the generation of waste (including wastewater) by applying the following waste management hierarchy in the order of priority as shown below:
 - (a) avoiding the production of waste
 - (b) minimising waste production
 - (c) reusing waste
 - (d) recycling waste
 - (e) recovering part of the waste for re-use
 - (f) treating waste to reduce the potentially degrading impacts
 - (g) disposing of waste in an environmentally sound manner.
- 2 The storage, treatment and disposal of waste materials from any development should be achieved without risk to health or impairment of the environment.
- 3 Development should avoid as far as practical, the discharge or deposit of waste (including wastewater) onto land or into any waters (including processes such as seepage, infiltration or carriage by wind, rain, sea spray, stormwater or by the rising of the water table).
- 4 Untreated waste should not be discharged to the environment, and in particular to any water body.
- 5 Development should include appropriately sized area to facilitate the storage of receptacles that will enable the efficient recycling of waste.
- 6 Development that involves the production and/or collection of waste and/or recyclable material should include designated collection and storage area(s) that are:
 - (a) screened and separated from adjoining areas
 - (b) located to avoid impacting on adjoining sensitive environments or land uses
 - (c) designed to ensure that wastes do not contaminate stormwater or enter the stormwater collection system
 - (d) located on an impervious sealed area graded to a collection point in order to minimise the movement of any solids or contamination of water

Salisbury Council
General Section
Waste

- (e) protected from wind and stormwater and sealed to prevent leakage and minimise the emission of odours
- (f) stored in such a manner that ensures that all waste is contained within the boundaries of the site until disposed of in an appropriate manner.

Wastewater

- 7 The disposal of wastewater to land should only occur where methods of wastewater reduction and reuse are unable to remove the need for its disposal, and where its application to the land is environmentally sustainable.
- 8 Wastewater lagoons should not be sited in any of the following areas:
 - (a) within land subject to a 1-in-100 year average return interval flood event
 - (b) within 50 metres of the top of the bank of a watercourse
 - (c) within 500 metres of the coastal high water mark
 - (d) where the base of the lagoon would be below any seasonal water table.
- 9 Artificial wetland systems for the storage of treated wastewater, such as wastewater lagoons, should be:
 - (a) sufficiently separated from adjoining sensitive uses to minimise potential adverse odour impacts
 - (b) sited and designed to minimise potential public health risks arising from the breeding of mosquitoes.

Waste Treatment Systems

- 10 Development that produces any sewage or effluent should be connected to a waste treatment system that complies with (or can comply with) the relevant public and environmental health legislation applying to that type of system.
- 11 The methods for, and siting of, effluent and waste storage, treatment and disposal systems should minimise the potential for environmental harm and adverse impacts on:
 - (a) the quality of surface and groundwater resources
 - (b) public health
 - (c) the amenity of a locality
 - (d) sensitive land uses.
- 12 Waste treatment should only occur where the capacity of the treatment facility is sufficient to accommodate likely maximum daily demands including a contingency for unexpected high flows and breakdowns.
- 13 Any on-site wastewater treatment system/ re-use system or effluent drainage field should be located within the allotment of the development that it will service.
- 14 A dedicated on-site effluent disposal area should not include any areas to be used for, or could be reasonably foreseen to be used for, private outdoor open space, driveways, car parking or outbuildings.
- 15 The spreading or discharging of treated liquid or solid waste onto the ground should only occur where the disposal area consists of soil and vegetation that has the capacity to store and use the waste without contaminating soil or surface or ground water resources or damaging crops.

- 16 Stock slaughter works, poultry processors, saleyards, piggeries, cattle feedlots, milking sheds, milk processing works, fish processing works, wineries, distilleries, tanneries and fellmongeries, composting works, waste or recycling depots and concrete batching works should have a wastewater management system that is designed so as not to discharge wastes generated by the premises:
- (a) into any waters
 - (b) onto land in a place where it is reasonably likely to enter any waters by processes such as:
 - (i) seepage
 - (ii) infiltration
 - (iii) carriage by wind, rain, sea spray, or stormwater
 - (iv) the rising of the watertable.
- 17 Winery waste management systems should be designed to ensure:
- (a) surface runoff does not occur from the wastewater irrigation area at any time
 - (b) wastewater is not irrigated onto waterlogged areas, land within 50 metres of a creek, or swamp or domestic or stock water bore, or land subject to flooding, steeply sloping land, or rocky or highly permeable soil overlaying an unconfined aquifer
 - (c) wastewater is not irrigated over an area which is within 50 metres of any residence on neighbouring land or 10 metres of any type of publicly owned land
 - (d) wastewater is released using low trajectory low pressure sprinklers, drip irrigators or agricultural pipe, and is not sprayed more than 1.5 metres into the air or in fine droplets if there is a potential for the spread of diseases from the wastewater
 - (e) stormwater run-off from areas which are contaminated with grape or grape products is drained to winery waste management systems during vintage periods
 - (f) stormwater from roofs and clean hard paved surfaces is diverted away from winery waste management systems and disposed of in an environmentally sound manner or used for productive purposes.

Noise and Air Emissions Overlay

Refer to the [Map Reference Tables](#) for a list of maps that relate to this overlay.

The following policies apply to the 'designated area' marked on the relevant *Overlay Map(s) – Noise and Air Emissions*.

INTERPRETATION

Where the Objectives and / or Principles of Development Control that apply in relation to this overlay are in conflict with the relevant General Objectives and/or Principles of Development Control in the Development Plan, the overlay will prevail.

OBJECTIVES

- 1 Protect community health and amenity from adverse impacts of noise and air emissions.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Noise and air quality sensitive development located adjacent to high noise and/or air pollution sources should:
 - (a) shield sensitive uses and areas through one or more of the following measures:
 - (i) placing buildings containing less sensitive uses between the emission source and sensitive land uses and areas
 - (ii) within individual buildings, place rooms more sensitive to air quality and noise impacts (e.g. bedrooms) further away from the emission source
 - (iii) erecting noise attenuation barriers provided the requirements for safety, urban design and access can be met
 - (b) use building design elements such as varying building heights, widths, articulation, setbacks and shapes to increase wind turbulence and the dispersion of air pollutants provided wind impacts on pedestrian amenity are acceptable
 - (c) locate ground level private open space, communal open space and outdoor play areas within educational establishments (including childcare centres) away from the emission source.

Salisbury Council
Overlay Section
Strategic Transport Routes Overlay

Strategic Transport Routes Overlay

Refer to the [Map Reference Tables](#) for a list of maps that relate to this overlay.

INTERPRETATION

Where the Objectives and / or Principles of Development Control that apply in relation to this overlay are in conflict with the relevant General Objectives and/or Principles of Development Control in the Development Plan, the overlay will prevail.

OBJECTIVES

- 1 Development that recognises the importance of strategic transport routes and does not impede traffic flow or create hazardous conditions for pedestrians, cyclists or drivers of vehicles, including emergency services vehicles.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development adjacent to a strategic transport route should:
 - (a) avoid the provision of parking on the main carriageway
 - (b) be accessible via service roads, where possible, that provide:
 - (i) parking off the main carriageway
 - (ii) a buffer from the main carriageway for pedestrian and cycle activity
 - (c) not impede the potential for overhead cabling and associated infrastructure to be established in an existing or proposed tram corridor.
- 2 Vehicular site access should not be provided along the main street frontage where an alternative access is available.
- 3 Development adjacent kerbside bus stops should be set back to provide sufficient space for indented bus bays with associated hard stand area, shelter and a 1.2 metre wide continuous accessible path behind the bus shelter.

Salisbury Council
Zone Section
Open Space Zone

Open Space Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A zone:
 - (a) in which the open space character is preserved to provide a visual contrast to the surrounding urban area
 - (b) comprising open space that accommodates a range of public and private activities in an open and natural setting, including:
 - (i) passive and active recreation land uses
 - (ii) habitat conservation and restoration
 - (iii) associated administrative, visitor and spectator facilities.
- 2 Land within the Metropolitan Open Space System (MOSS) contributing to regional open space networks and providing an open, natural and rural character accommodating low-scale uses such as non-intensive agriculture
- 3 Public land within the MOSS that provides for recreation areas and facilities, sporting facilities and conservation of the open, natural character.
- 4 A linear park that:
 - (a) provides an open space corridor across Greater Adelaide/Regional South Australia
 - (b) promotes the use, awareness and preservation of the environment and enhancement of natural or semi natural habitats for the movement of wildlife and conservation of biodiversity.
- 5 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The zone provides for a range of passive and active recreation land uses, and associated spectator facilities and administrative functions.

It will provide for the establishment and maintenance of a linear open space and biological corridor based on the existing watercourse channel, associated floodplains and vegetation.

Pedestrian, cycle and recreation facilities, such as drinking fountains and barbecues, will be incorporated into the Linear Park; however, facilities for sports competitions or events are inappropriate.

Landscaping should ensure that any remnant native vegetation is incorporated into the landscape design and that locally indigenous vegetation is utilised wherever possible.

PRINCIPLES OF DEVELOPMENT CONTROL**Land Use**

- 1 The following forms of development are envisaged in the zone:
 - conservation work
 - farming
 - outbuilding associated with open space maintenance
 - playground
 - recreation area
 - sporting club facility
 - structure associated with a public facility such as car parking, picnic/barbeque area, shelter and toilet
 - toilet block and barbeque facility
- 2 Development listed as non-complying is generally inappropriate.
- 3 Publicly owned land within the MOSS should be used for any of the following:
 - (a) to provide natural or landscaped open space using locally indigenous plant species
 - (b) to accommodate a range of public recreation, sporting and institutional facilities and uses
 - (c) to accommodate stormwater retention and management
 - (d) to conserve and restore areas of remnant native vegetation and wildlife habitats and corridors
 - (e) to conserve sites of scientific, cultural or heritage interest
 - (f) for revegetation purposes using locally indigenous plant species
 - (g) to provide a buffer to adjoining areas of conservation significance.
- 4 Privately owned land within the MOSS should be used for any of the following:
 - (a) rural activities and agriculture (but not intensive animal keeping)
 - (b) low-impact sporting facilities
 - (c) to accommodate a range of public recreation, sporting and institutional facilities and uses
 - (d) conservation purposes.
- 5 Development should allow for unstructured passive and active recreation.
- 6 Development within the area marked 'The Paddocks' on [Concept Plan Map Sal/17 - The Paddocks Open Space Zone](#) should:
 - (a) be comprised primarily of small-scale public and private recreation facilities
 - (b) be maintained as a visual buffer between different parts of the metropolitan area
 - (c) maintain existing pedestrian and cycle linkages
 - (d) establish future pedestrian and cycle linkages
 - (e) provide opportunities for future recreational activities.

Salisbury Council
Zone Section
Open Space Zone

Form and Character

- 7 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 8 Development, unless specified elsewhere within the zone or policy areas, should only occur where it is integral to the aesthetic, drainage or recreation function of the zone.
- 9 Buildings should be:
 - (a) restricted in size and number
 - (b) sited so as not to detract from the open natural character of the zone
 - (c) be well set-back from public roads
 - (d) be clustered, where practicable, to ensure that the majority of any site remains open in appearance
 - (e) generally be of dark natural colours such as brown and green so as to be unobtrusive and not detract from natural elements of the landscape
 - (f) constructed of materials which blend with the landscape.
- 10 Development should ensure co-ordinated design with an emphasis on the creation of pedestrian areas.
- 11 Landscaped buffers should be provided around the perimeter of recreation or sporting facilities.
- 12 Landscaping should comprise locally indigenous species and incorporate existing remnant vegetation.
- 13 Vehicular access or crossings should not occur through the Linear Park.
- 14 Development should ensure that public access to the Linear Park is retained and enhanced for cyclists and pedestrians.
- 15 Development, landscaping, and paths for pedestrians and cyclists should:
 - (a) take into account the changing flow regime and width of waterways and
 - (b) be constructed of permeable material where practical to reduce stormwater runoff.
- 16 Advertisements should be restricted to user's identity, property, name and address.
- 17 Advertisements should:
 - (a) not include portable, flashing or moving displays
 - (b) not wholly or partly consist of bunting, streamers, flags, wind vanes and the like
 - (c) where internally illuminated, be unobtrusive and not be conspicuous from residential properties.

Land Division

- 18 Land division should not be undertaken except where:
 - (a) it will facilitate the development of envisaged uses in the zone
 - (b) no additional allotments are created.

Salisbury Council
Zone Section
Open Space Zone
Recreation Policy Area 15

Recreation Policy Area 15

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 A policy area accommodating sporting, entertainment, cultural and recreational activities and associated spectator and administrative facilities.
- 2 Development of integrated recreational areas and facilities that accommodate a range of activities accessible to the community.
- 3 Buildings, facilities and car parks located and designed to blend in with existing or additional trees, vegetation and landscaping.
- 4 Development within the area marked 'Community Purposes' on [Concept Plan Map Sal/19 - Pooraka Open Space Recreation](#) which is comprised of community, recreational, sporting, educational and religious facilities.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:
 - car parking
 - clubroom associated with a sports facility
 - entertainment, cultural and exhibition facility
 - golf course
 - indoor and outdoor recreation facility
 - lighting for night use of facilities
 - office associated with community or recreation facility
 - playground
 - shops or groups of shops ancillary to recreation development
 - showground
 - sports ground and associated facility
 - special event
 - spectator and administrative facilities ancillary to recreation development
 - swimming pool.
- 2 A shop or group of shops should only be developed where:
 - (a) it is ancillary to recreation and sport development
 - (b) the total gross leasable area is less than 80 square metres.
- 3 Development within the area marked 'Community/Recreation' on [Concept Plan Map Sal/19 - Pooraka Open Space Recreation](#) should be comprised of community, recreational, sporting, educational and religious facilities.
- 4 Development within the area marked 'Community/Recreation' on [Concept Plan Map Sal/19 - Pooraka Open Space Recreation](#) should contribute to the visual separation of the urban regions of Salisbury and Enfield, and retain a predominantly open space character derived primarily from parklands, public recreation facilities of an open nature, and landscaping. Buildings, structures, advertisements and hard-paved areas should be unobtrusive, particularly as viewed from public roads.

Salisbury Council
Zone Section
Open Space Zone

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of development	Exceptions
Advertisement and/or advertising hoarding	Except where it complies with all of the following: (a) it does not move, rotate or incorporate flashing lights (b) it does not wholly or partly consist of bunting, streamers, flags, wind vanes and the like (c) if attached to a building, it does not wholly or partly extend above the top of the wall or the fascia.
Amusement machine centre	
Bus depot	
Bus station	
Caravan park	
Consulting room	
Crematorium	
Dairy	
Dwelling	Except where it is ancillary to and in association with an envisaged development.
Educational establishment	
Electricity substation	
Fuel depot	
Hall	Except where it is located outside of the Recreation Policy Area 15
Horticulture	
Hospital	
Hotel	Except on Allotment 200 in Deposited Plan 41172
Industry	
Intensive animal keeping	
Land division	Except where no additional allotments are created partly or wholly within the zone.
Motel	
Motor repair station	
Nursing home	

Form of development	Exceptions
Office	Except where in association with recreation and sporting facilities.
Petrol filling station	
Place of worship	Except where it is located outside of Landscape Buffer Policy Area 14.
Pre-school	
Prescribed mining operations	
Public service depot	
Residential flat building	
Restaurant	
Road transport terminal	
Service trade premises	
Shop or group of shops	
Stock sales yard	
Stock slaughter works	
Store	
Tourist accommodation	
Warehouse	
Waste reception, storage, treatment or disposal	
Welfare institution	
Wrecking yard	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Salisbury Council
Zone Section
Primary Production Zone

Primary Production Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 The long term continuation of primary production.
- 2 Economically productive, efficient and environmentally sustainable primary production.
- 3 Allotments of a size and configuration that promote the efficient use of land for primary production.
- 4 Protection of primary production from encroachment by incompatible land uses and protection of scenic qualities of rural landscapes.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - bulk handling and storage facility
 - commercial forestry
 - dairy farming
 - farming
 - horticulture
 - tourist accommodation (including through the diversification of existing farming activities and conversion of farm buildings).
- 2 Development listed as non-complying is generally inappropriate.
- 3 Industry and warehousing should only be developed if it supports primary production, processing, storage and distribution of local primary produce or products produced on the same site, and should be developed where:
 - (a) it has a direct relationship with primary production
 - (b) it is unlikely to limit or inhibit the use of adjoining land for primary production
 - (c) the particular use requires a site in proximity to a particular natural resource or other product or materials sourced from the locality
 - (d) it will not result in the alienation of land or water resources identified as significant for primary production or ecological reasons
 - (e) the use would be inappropriate within a township
 - (f) the capacity of the infrastructure, including roads, is capable of supporting the use without detriment to existing users.
- 4 A dwelling should only be developed if:
 - (a) there is a demonstrated connection with farming or other primary production

- (b) the location of the dwelling will not inhibit the continuation of farming, other primary production or other development that is in keeping with the provisions of the zone
 - (c) it is located more than 500 metres from an existing intensive animal keeping operation unless used in association with that activity
 - (d) it does not result in more than one dwelling per allotment.
- 5 Tourist accommodation should not be converted to dwellings and should be designed to preclude the conversion of buildings into dwellings such as through shared facilities, common utility services, grouped accommodation and/or shared parking.
- 6 A shop should be:
- (a) ancillary to primary production or processing uses or tourist accommodation or other tourist development
 - (b) located on the same site as the primary use.

Form and Character

- 7 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 8 Development should not occur within 500 metres of a National Park, Conservation Park, Wilderness Protection Area or significant stands of native vegetation if it will increase the potential for, or result in, the spread of pest plants.
- 9 Development should provide an access way of at least 3 metres wide that provides access for emergency vehicles to the rear of the allotment.
- 10 Buildings should primarily be limited to farm buildings, a detached dwelling associated with primary production on the allotment and residential outbuildings that are:
- (a) grouped together on the allotment and set back from allotment boundaries to minimise the visual impact of buildings on the landscape as viewed from public roads
 - (b) screened from public roads and adjacent land by existing vegetation or landscaped buffers.
- 11 Buildings should be set back at least 30 metres from the top of the banks of the Little Para River west of Port Wakefield Road.
- 12 Alterations or additions to the hotel/motel complex at Bolivar should:
- (a) be limited to the existing site
 - (b) be designed to complement existing buildings on the site
 - (c) have minimal visual impact from Port Wakefield Road through the use of extensive landscaping and tree planting.
- 13 Horse keeping (not associated with stables) should only occur on allotments of 4 hectares or more and be designed to ensure that adequate ground cover is able to be maintained at all times to avoid soil degradation.
- 14 Horse keeping should not occur on allotments of less than 4 hectares, except in combination with stabling.
- 15 Keeping or stabling of horses and other animals, and associated development, should be undertaken with the minimum impact upon the environment and be designed to avoid the following:

Salisbury Council
Zone Section
Primary Production Zone

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions
Advertisement and/or advertising hoarding	Advertisement and/or advertising hoarding where the development achieves at least one of (a) or (b): <ul style="list-style-type: none"> (a) is adjacent to a road with a speed limit of less than 80 km/h (b) has an advertisement area of 2 square metres or less and achieves all of the following: <ul style="list-style-type: none"> (i) the message contained thereon relates entirely to a lawful use of land (ii) the advertisement is erected on the same allotment as the use it seeks to advertise (iii) the advertisement will not result in more than two advertisements on the allotment.
Amusement machine centre where it is located within the Aircraft Noise Policy Area 16	
Bed and Breakfast	Except where the maximum number of guests which can be accommodated does not exceed five.
Bus depot where it is located within the Aircraft Noise Policy Area 16	
Bus station where it is located within the Aircraft Noise Policy Area 16	
Caravan Park	
Community centre where it is located within the Aircraft Noise Policy Area 16	
Consulting room	
Dwelling	Except where one or more of the following applies: <ul style="list-style-type: none"> (a) it is located outside of the Horticulture Policy Area 17 and Aircraft Noise Policy Area 16 and it does not result in more than one dwelling on an allotment (b) it is located within the Aircraft Noise Policy Area 16 and it achieves one of the following: <ul style="list-style-type: none"> (i) it is an addition or alteration to a detached dwelling that existed at 20 October 1988 (ii) it is a detached dwelling, it does not result in more than one dwelling on an allotment and it is located on an allotment that existed at 20 October 1988 in those portions of Allotment 43 of FP114404, Allotment 1 of DP20833, Allotment 2 of DP20833, Allotment 41 of FP114402, Allotment 18 of FP102000, Allotment 4 of FP7982 and Allotment 103 of DP38939

Form of Development	Exceptions
Shop or group of shops	Except where it is located outside of the Aircraft Noise Policy Area 16 and Horticulture Policy Area 17 and it achieves all of the following: <ul style="list-style-type: none"> (a) it is ancillary to and in association with a petrol filling station or a primary production or tourism development (b) it has a maximum gross leasable area of no more than 150 square metres.
Showground where it is located within the Aircraft Noise Policy Area 16	
Stadium	
Stock slaughter works where it is located within the Aircraft Noise Policy Area 16	
Store where it is located within the Aircraft Noise Policy Area 16	
Theatre in the Aircraft Noise Policy Area 16	
Warehouse	
Waste reception, storage, treatment or disposal	
Wrecking yard	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is non-complying) are designated:

Category 1	Category 2
The following forms of development located outside of the Aircraft Noise Policy Area 16 :	
<ul style="list-style-type: none"> (a) commercial forestry (b) farm building (c) farming (d) horticulture (e) horticultural building (f) stable. 	

Table Sal/2 - Off Street Vehicle Parking Requirements

The following vehicle parking requirements do not apply:

- (a) to the Mixed Use (Bulky Goods, Entertainment, Leisure) Zone except where the form of development is light industry whereby the rates for Industry, warehouse, stores are applicable
- (b) to development that is subject to the requirements in [Table Sal/2A - Off Street Vehicle Parking Requirements for Designated Areas](#).

Form of Development	Number of Required Car Parking Spaces
Accommodation	
Aged Care / retirement home	1 space per unit
Serviced apartment	1 space per unit plus 1 space per employee
Motel	1 space per unit
Commercial	
Bulky goods outlet	3 spaces per 100 square metres of gross leasable floor area
Cinema	1 space per 4 cinema seats
Hotel	1 space per 2 square meters of floor area available to the public
Public bar	1 space per 6 square metres of floor area available to the public
Lounge or beer garden	1 space per 6 square metres of floor area available to the public
Gaming room	1 space per 2 machines
Office	1 space per 25 square metres, with a minimum of 4 spaces per office
Restaurant	Greater of 1 space for every 3 seats or 1 space for every 15 square metres of dining area
Service trade premises	3 spaces per 100 square metres
Shop	7 spaces per 100 square metres of gross leasable area for shops outside of centre zones 5 spaces per 100 square metres of gross leasable area for shops within centre zones
Community/civic	
Child care centre	1 space per 4 children
Community centre	10 spaces per 100 square metres of total floor area
Library	4 spaces per 100 square metres
Place of worship	Greater of 1 space for every 3 seats or every 3 attendees
Dwellings	

Salisbury Council

Table Section

Table Sal/3 - Off Street Bicycle Parking Requirements

Table Sal/3 - Off Street Bicycle Parking Requirements

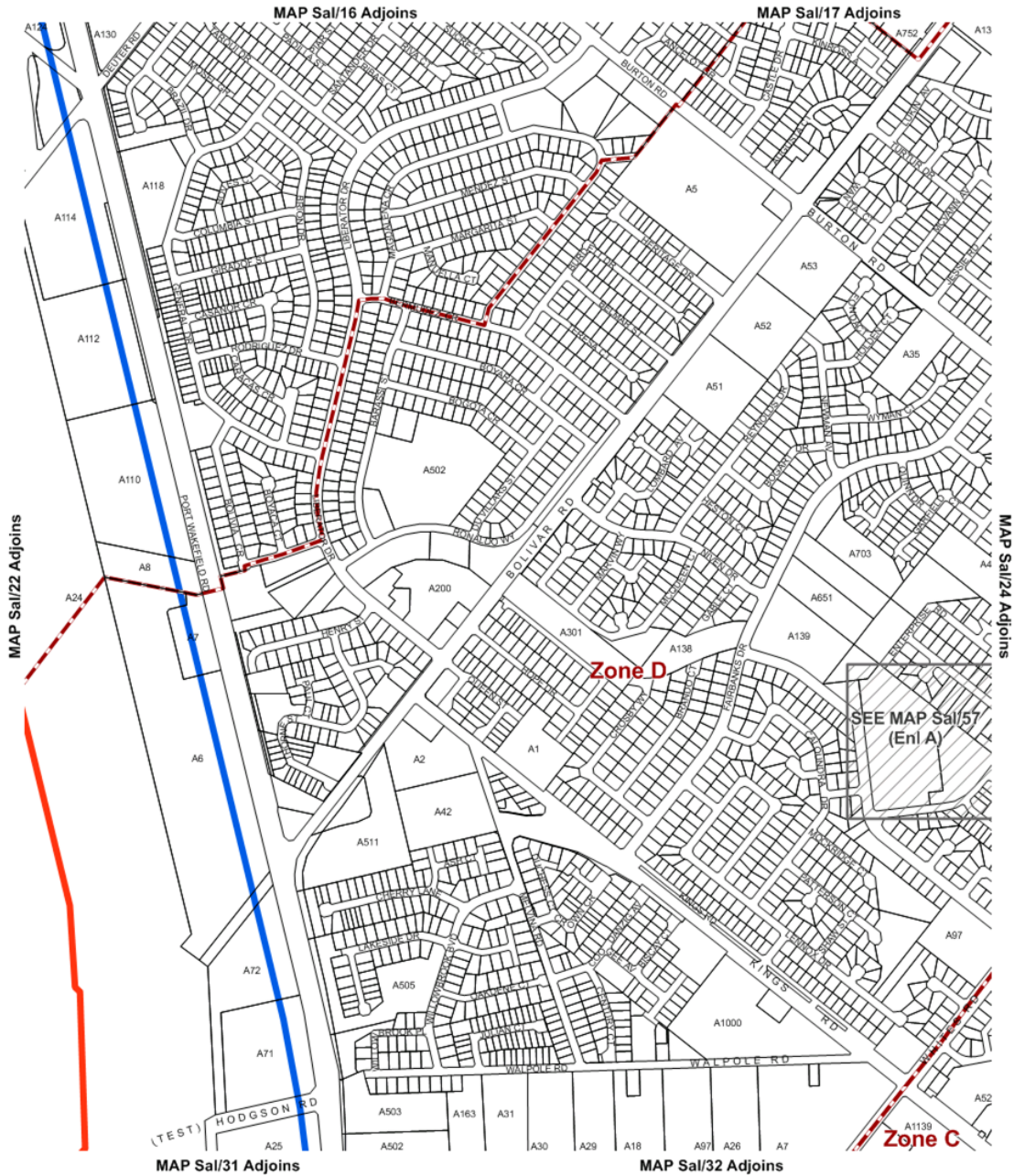
Form of development	Employee/resident (bicycle parking spaces)	Visitor/shopper (bicycle parking spaces)
Residential component of multi-storey building/residential flat building	1 for every 4 dwellings	1 for every 10 dwellings
Office	1 for every 200 square metres of gross leasable floor area	2 plus 1 per 1000 square metres of gross leasable floor area
Shop	1 for every 300 square metres of gross leasable floor area	1 for every 600 square metres of gross leasable floor area
Tourist accommodation	1 for every 20 employees	2 for the first 40 rooms plus 1 for every additional 40 rooms



Item 8.1.1 - Attachment 4 - Extract of Relevant Development Plan Provisions and Location Maps (Consolidated 4 April 2019)



Item 8.1.1 - Attachment 4 - Extract of Relevant Development Plan Provisions and Location Maps (Consolidated 4 April 2019)



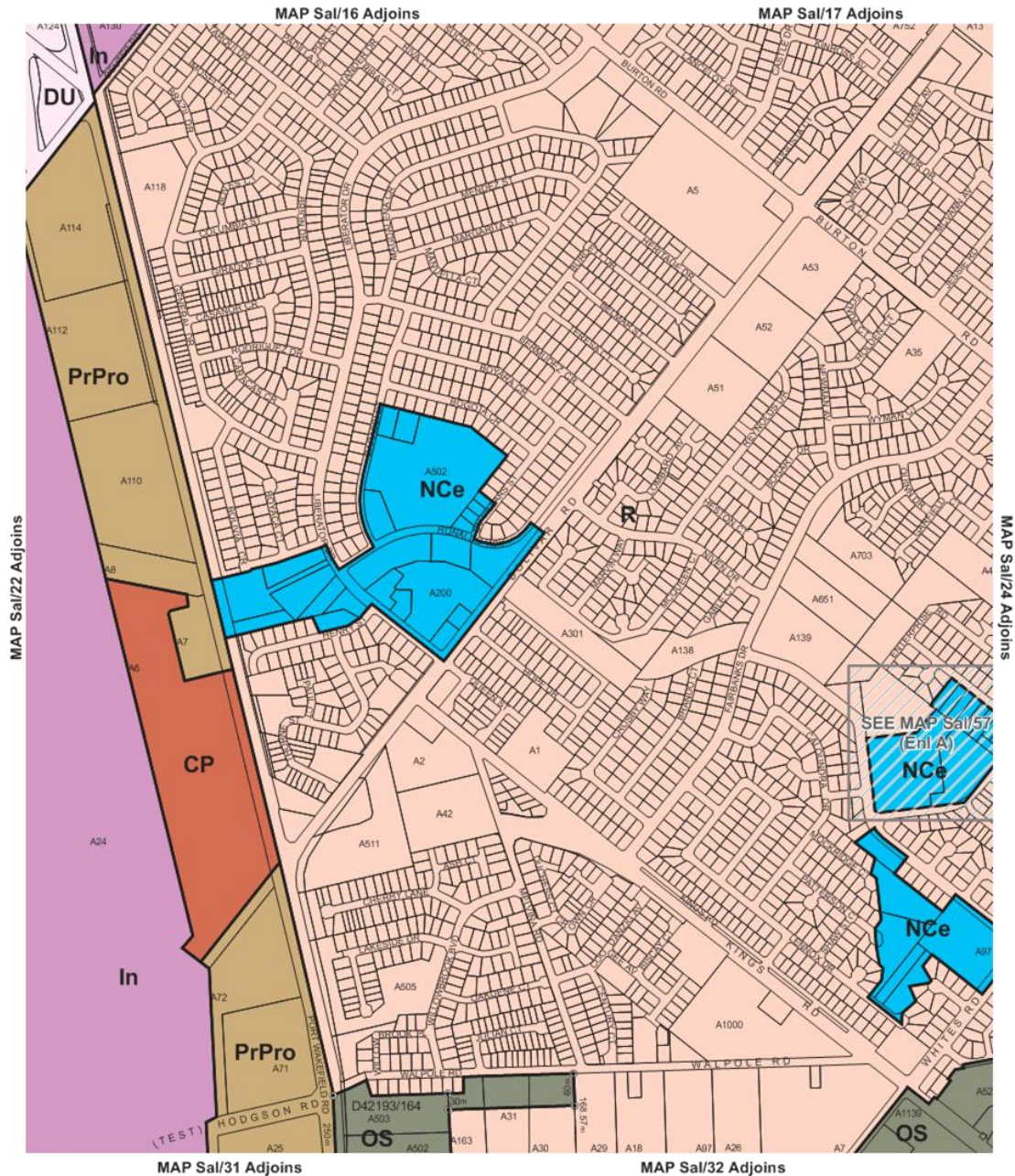
Airport Building Heights
Zone C All Structures Exceeding 15 metres above existing ground level
Zone D All Structures Exceeding 45 metres above existing ground level

- Epic Energy Gas Pipeline
- South East Australia Gas Pipeline
- Airport Building Heights

Overlay Map Sal/23 DEVELOPMENT CONSTRAINTS

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Item 8.1.1 - Attachment 4 - Extract of Relevant Development Plan Provisions and Location Maps (Consolidated 4 April 2019)



See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

- Zones**
- CP Caravan and Tourist Park
 - DU Deferred Urban
 - In Industry
 - NCe Neighbourhood Centre
 - OS Open Space
 - PrPro Primary Production
 - R Residential
 - Zone Boundary



Zone Map Sal/23

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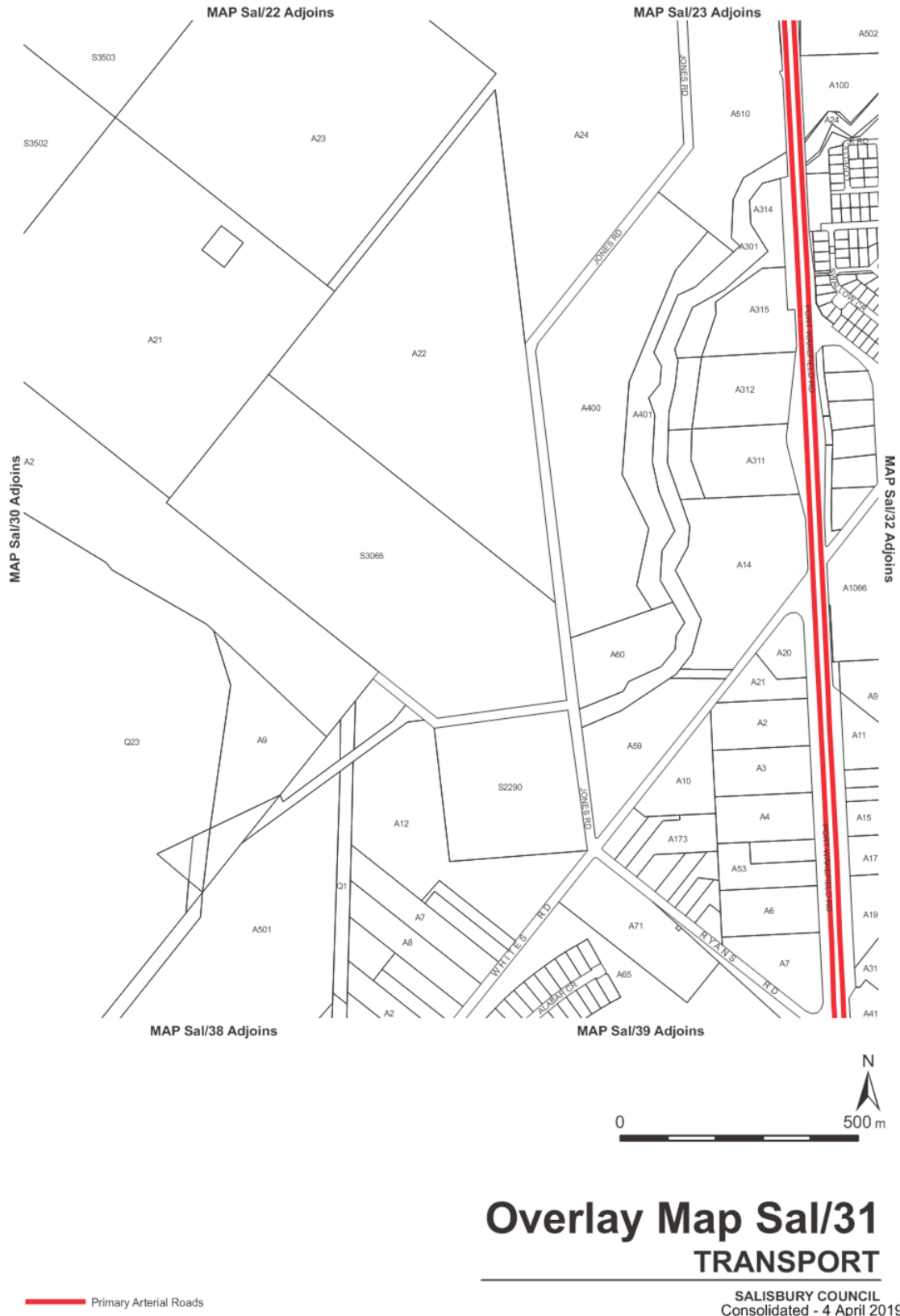


- Local Reserves
- Conservation Park
- Waterbodies

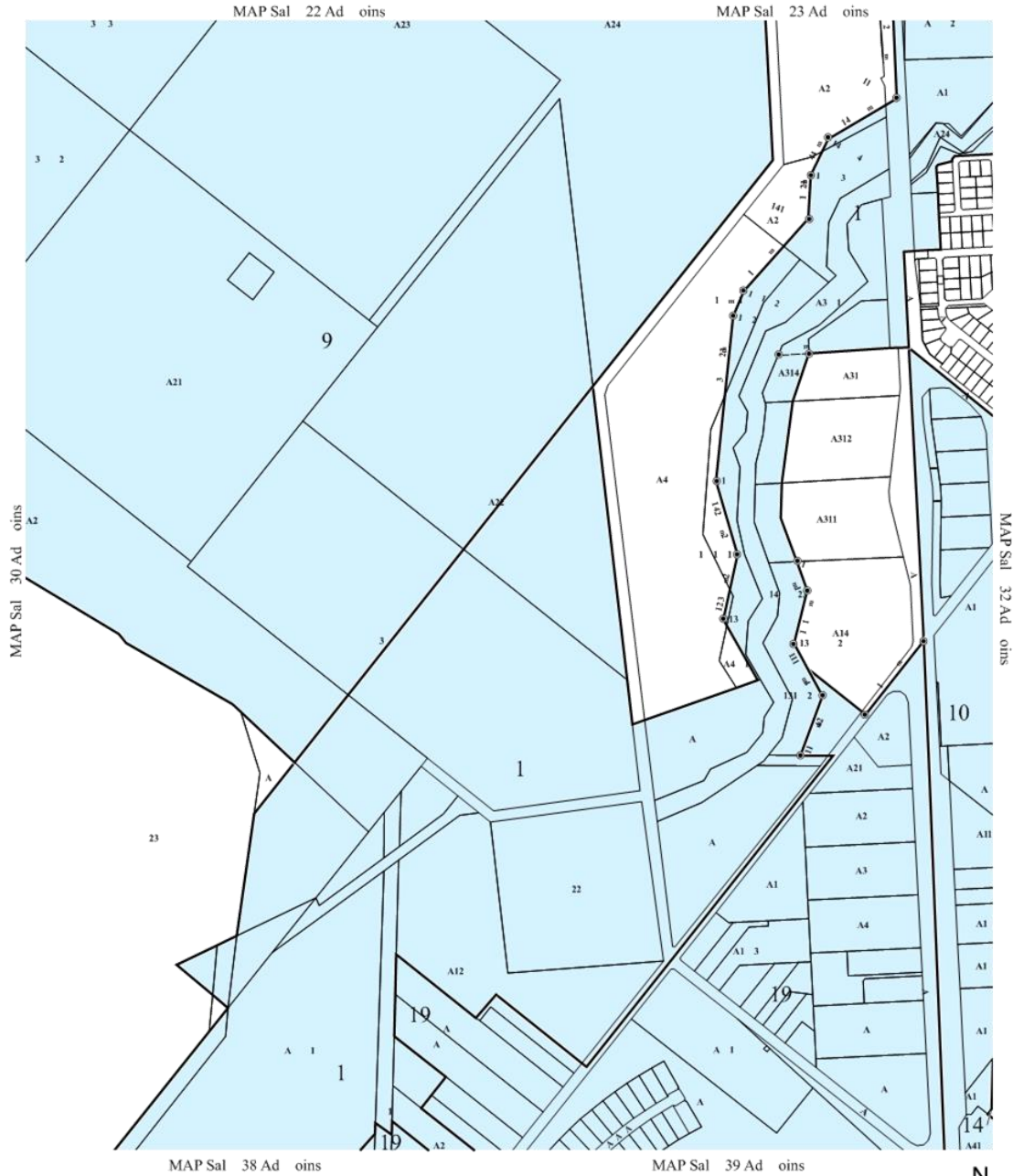
Location Map Sal/31

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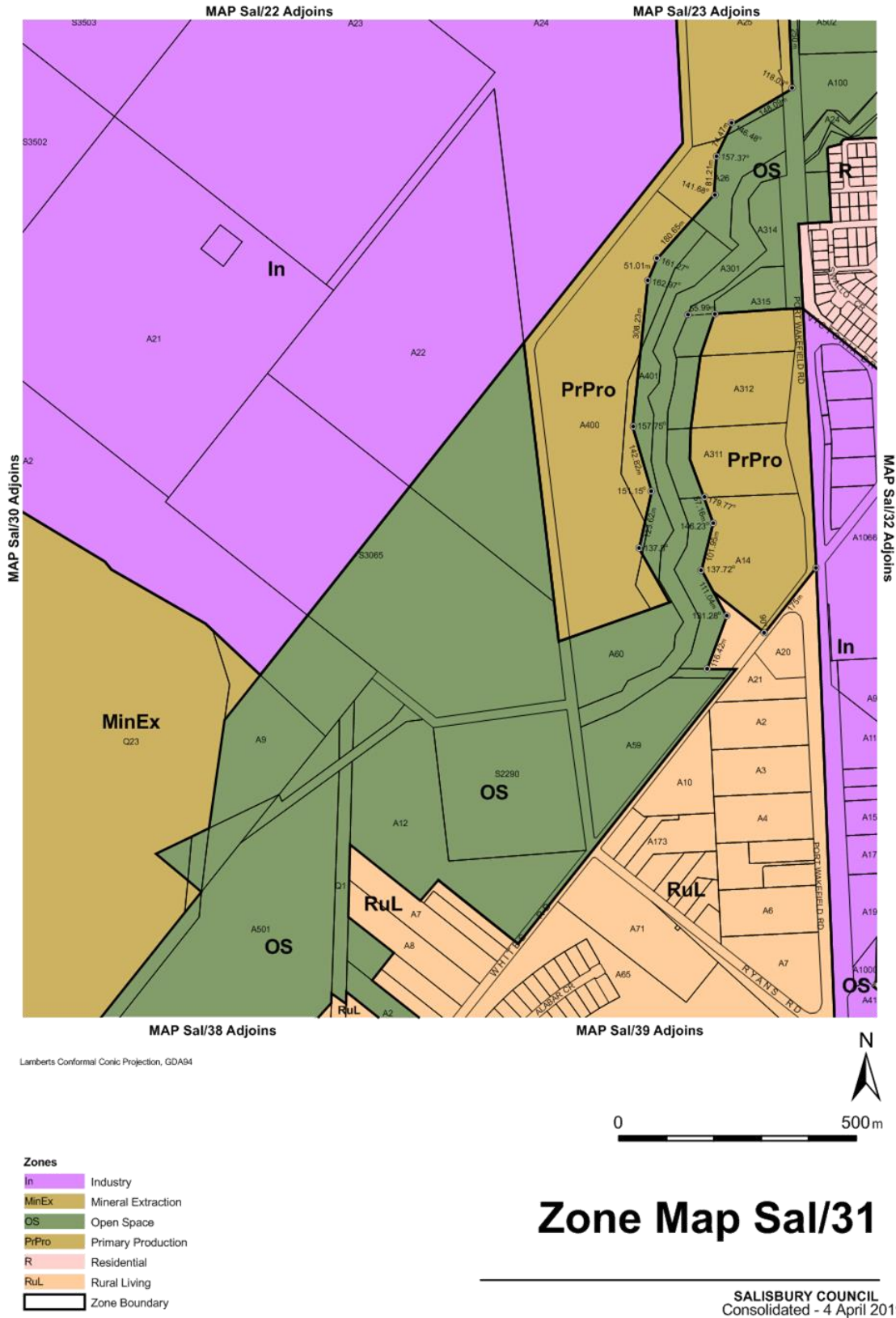
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Policy Area Map Sal 31

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